

File No.: 04-1000-20-2023-131

April 20, 2023

s.22(1)

Dear s.22(1)

Re: **Request for Access to Records under the Freedom of Information and Protection of Privacy Act (the "Act")**

I am responding to your request of March 6, 2023 under the ***Freedom of Information and Protection of Privacy Act*** for:

1. **Records related to the installation, maintenance, inspection, repair, and hazard warning to the public regarding the sign post sleeve on the sidewalk at the NW corner of Smithe and Burrard Street; and**
2. **Record of the City's general policy documents regarding the marking of hazards on City property.**

**Date range: Date of installation to March 6, 2023.**

All responsive records are attached. Some information in the records has been severed (blacked out) under s.15(1)(l) and s.22(1) of the Act. You can read or download these sections here: [http://www.bclaws.ca/EPLibraries/bclaws\\_new/document/ID/freeside/96165\\_00](http://www.bclaws.ca/EPLibraries/bclaws_new/document/ID/freeside/96165_00).

Under section 52 of the Act, and within 30 business days of receipt of this letter, you may ask the Information & Privacy Commissioner to review any matter related to the City's response to your FOI request by writing to: Office of the Information & Privacy Commissioner, [info@oipc.bc.ca](mailto:info@oipc.bc.ca) or by phoning 250-387-5629.

If you request a review, please provide the Commissioner's office with: 1) the request number (#04-1000-20-2023-131); 2) a copy of this letter; 3) a copy of your original request; and 4) detailed reasons why you are seeking the review.

Yours truly,

*[Signed by Cobi Falconer]*

**Cobi Falconer, MAS, MLIS, CIPP/C**  
**Director, Access to Information & Privacy**  
[cobi.falconer@vancouver.ca](mailto:cobi.falconer@vancouver.ca)

453 W. 12th Avenue Vancouver BC V5Y 1V4

If you have any questions, please email us at [foi@vancouver.ca](mailto:foi@vancouver.ca) and we will respond to you as soon as possible. Alternatively, you can call the FOI Case Manager at 604-871-6584.

Encl. (Response package)

:pm

**From:** "Traffic Operations" <s.15(1)(l)>  
**To:** "Sibley, Marc" <Marc.Sibley@vancouver.ca>  
"Hatton, Rommel" <Rommel.Hatton@vancouver.ca>  
"Sandhu, Bhapinder" <Bhapinder.Sandhu@vancouver.ca>  
**CC:** "Traffic - Temporary Permit Decals" <s.15(1)(l)>  
"Sun, Serena" <Serena.Sun@vancouver.ca>

**Date:** 10/26/2021 3:53:00 PM

**Subject:** CEM1345836; Smithe Street Bikeway 2021 - Smithe and Haro St  
DOC/2021/223214 + 223217

**Attachments:** ENG - TDE - Form 1 - Smithe Bikeway 2021 - Hornby to Richards -  
2018-32 - 2021-09-07.tr5  
ENG - TDE - Form 1 - Smithe Bikeway 2021 - Hornby to Thurlow -  
2021-15 - 2021-09-07.tr5

WO#1349936 + WO#1349938 have been created for the attached FORM 1 requests.  
Paperwork will be delivered to TO Office shortly for review.  
Charge #CEM1345836 for Both FORM 1s as per our previous conversations.

Oliver Thomas

**Traffic, Electrical Operations & Design Branch**

City of Vancouver | 604.871.6778

**From:** Sun, Serena <Serena.Sun@vancouver.ca>  
**Sent:** Monday, October 25, 2021 9:34 AM  
**To:** Sandhu, Bhapinder <Bhapinder.Sandhu@vancouver.ca>; Hatton, Rommel  
<Rommel.Hatton@vancouver.ca>; Sibley, Marc <Marc.Sibley@vancouver.ca>; Leclair, Johnny  
<Johnny.Leclair@vancouver.ca>; Traffic Operations s.15(1)(l)  
**Cc:** Islam, Tasnuba <Tasnuba.Islam@vancouver.ca>; Gould, Brian <Brian.Gould@vancouver.ca>; Smith,  
Laura <Laura.Smith@vancouver.ca>; Paccani, Vince <vince.paccani@vancouver.ca>  
**Subject:** CEM1345836; Smithe Street Bikeway 2021 - Smithe and Haro St DOC/2021/223214 + 223217

Hi Traffic Operations,

Please see attached for approved form 1s for Smithe Bikeway.

Regards,  
Serena Sun  
Geometric Designer, Transportation Design | City of Vancouver  
604 871 6596

[serena.sun@vancouver.ca](mailto:serena.sun@vancouver.ca)











Block(s):	900 & 1000 Smithe 1000 & 1100 Haro	Form 1 VanDocs #	DOC/2021/223217
Street(s):	Smithe St Haro St	Site Investigation(s):	DOC/2021/182438
Project/Program:	Smithe Bikeway 2021	Account #:	CER-00250-KK-02-CC-05
Drawing Type(s):	<input checked="" type="checkbox"/> Sign Plan <input type="checkbox"/> Geometric <input type="checkbox"/> Paint Plan		
Provincial Heritage Pre-Contact Site?*	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No		

\* If "yes", special instructions are required before any digging can occur as it may be an archeological site. Please verify with EoR or Designer.

Issuance Log (add newer revisions to bottom of table)

Revision	Minor Revision?*	Drawing VanDocs #	Date (YYYY-MM-DD)	Description of Revision
02.1	Yes	DOC/2021/248022	2021-10-08	GP revision 02

\* [Y]es / [N]o. If revisions are not minor (at the discretion of EoR), new branch heads /director approval signatures are required.

Design Rationale

Bi-directional protected bike lane on the north side of Smithe St from Richards to Haro. Include protected intersection at Hornby and Thurlow intersection (connection to Haro Bikeway). Exclude most scope at Richards/Smithe (part of Richards Bikway). Signal modifications for protected-only rights and contra-flow bikes. <ul style="list-style-type: none"><li>• Upgrade Smithe Street for all ages and abilities cycling in both directions</li><li>• Improve safety by addressing conflicts with through and turning drivers</li><li>• Design for cycling connections to build a AAA network</li><li>• Maintain people-moving capacity</li><li>• Balance parking with active transportation upgrades</li></ul>
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Design Acceptance and Approval to Install

Signature Req'd	Approval Level	Signature, Date (YYYY-MM-DD)	Phone
Yes	Author	Serena Sun\ Pradeep Paudel (2021-09-17)	8-6596
Yes	Supervisor/Engineer of Record	Brian Gould, 2021-09-18	8(71)-6277
Yes	Parking Management		-
Yes	Traffic & Data Management	Winston Chou, 2021-10-05	-
Yes	Transportation Design	John Turecki, 2021-09-23	-
<input type="checkbox"/>	Transportation Planning		-
Yes	Director of Transportation	Paul Storer, 2021-10-15	-

Operations

Installation by / coordinated with:

<input checked="" type="checkbox"/> Traffic and Electrical Ops	<input checked="" type="checkbox"/> Parking Ops	<input type="checkbox"/> Other:
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Instructions			
<u>Traffic Control Signs</u> Please install, remove and relocate traffic control signs as per Sign Plan Drawing Rev 02: DOC/2021/248022			
<u>I.R.S. Bollards</u> Please install yellow I.R.S. bollards with reflective tape as per Geometric Plan Drawing Rev 02: DOC/2021/232802			
<u>Parking Meters</u> Please remove parking meters as per Geometric Plan Drawing Rev 02: DOC/2021/232802			
Inventory			
Please refer to the Sign Code Inventory on the Sign Plan Drawing Rev 02: DOC/2021/248022			

Installed by:	Date (YYYY-MM-DD)	Field Review by:	Date (YYYY-MM-DD)
Installed per Design?	<input type="checkbox"/> Yes <input type="checkbox"/> No <i>Note changes and reasons on drawing or in instructions box above.</i>		



**From:** "Traffic Operations" <s.15(1)(l)>  
**To:** "Sibley, Marc" <Marc.Sibley@vancouver.ca>  
**CC:** "Sibley, Marc ;" <Marc.Sibley@vancouver.ca>  
"Hatton, Rommel ;" <Rommel.Hatton@vancouver.ca>  
"Sandhu, Bhapinder" <Bhapinder.Sandhu@vancouver.ca>  
**Date:** 2/13/2023 9:10:57 AM  
**Subject:** FW: IRS WO 1661762 - Empty sign sleeve - 1000 Smithe N/S  
**Attachments:** TEOD Work Slip (52).pdf

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Forwarding a PDF of WO#1661762 for your review.  
Any issues let us know.

Thanks,  
Oliver Thomas  
Traffic, Electrical Operations & Design Branch City of Vancouver  
s.15(1)(l)

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**From:** Sibley, Marc <Marc.Sibley@vancouver.ca>  
**Sent:** Monday, February 13, 2023 8:52 AM  
**To:** Hatton, Rommel <Rommel.Hatton@vancouver.ca>; Traffic Operations <s.15(1)(l)>  
**Cc:** Sandhu, Bhapinder <Bhapinder.Sandhu@vancouver.ca>  
**Subject:** RE: IRS WO 1661762 - Empty sign sleeve - 1000 Smithe N/S

As per crew notes attached

Traffic Ops crew ES/MR – ? E1107 have identified this sleeve as a " ? Meter " ? sleeve

Traffic Ops crew have eliminated the trip hazard by placing a pipe 10' ? in the sleeve

Please forward to Parking Operations

Thanks  
-Marc

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**From:** Hatton, Rommel <Rommel.Hatton@vancouver.ca>  
**Sent:** Friday, February 10, 2023 7:35 AM  
**To:** Traffic Operations <s.15(1)(l)>  
**Cc:** TO Supervisor - DL <s.15(1)(l)>  
**Subject:** FW: IRS WO 1661762 - Empty sign sleeve - 1000 Smithe N/S

work order has been assigned to traffic operations crew #es/rs/mr (e1107) to investigate

 Please consider the environment before printing this email 

**From:** Traffic Operations << s.15(1)(l) >>  
**Sent:** Thursday, February 09, 2023 3:00 PM  
**To:** Sibley, Marc << [Marc.Sibley@vancouver.ca](mailto:Marc.Sibley@vancouver.ca) >> ; Hatton, Rommel << [Rommel.Hatton@vancouver.ca](mailto:Rommel.Hatton@vancouver.ca) >>  
**Cc:** Sandhu, Bhapinder << [Bhapinder.Sandhu@vancouver.ca](mailto:Bhapinder.Sandhu@vancouver.ca) >>  
**Subject:** FW: IRS WO 1661762 - Empty sign sleeve - 1000 Smithe N/S

Paper copy of WO#1661762 printed to TO Office for review.

Thanks,  
Oliver Thomas  
Traffic, Electrical Operations & Design Branch City of Vancouver  
s.15(1)(l)

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**From:** Street Operations (Internal Use) << s.15(1)(l) >>  
**Sent:** Thursday, February 9, 2023 2:53 PM  
**To:** Traffic Operations << s.15(1)(l) >>  
**Cc:** Street Operations (Internal Use) << s.15(1)(l) >>  
**Subject:** FW: IRS WO 1661762 - Empty sign sleeve - 1000 Smithe N/S

Hello,

Please see IRS WO 1661762 identifying a pole sleeve with no pole in it 13m west of Pole .5/10 on Smithe St on the north side. Crews have placed a cone over the sleeve and are requesting referral to Traffic Ops.

Thanks!

**Tyler Fahmi**  
Street Operations | National Yard  
City of Vancouver | 604-871-6494



# TEOD Work Slip

<b>SAP Network #</b>	<b>Address</b>		<b>Lane</b>
EER1154468	1000 - 1099 SMITHE ST		
<b>Work Order #</b>	<b>WO Short Description</b>		<b>Reason</b>
1661762	Boulevard Hazard		
<b>Activity Type</b>	<b>Sub Activity Type</b>	<b>Problem</b>	<b>Priority</b>
Boulevard Hazard			[ ] Emergency [ ] High

## **SAFETY WARNING:**

<b>Group Project</b>	<b>Ref #1</b>	EER1154468 Standing Network #	<b>Responsibility</b>	<b>Parent SAP Network #</b>	
	<b>Ref #2</b>		RO		
<b>Service Request #</b>	<b>Incident Date</b>	<b>311 Case #</b>	<b>Contact Name</b>	<b>Contact #</b>	
<b>Asset Category</b>	<b>Asset ID</b>	<b>Status</b>	<b>Asset Area</b>	<b>Sub Area</b>	<b>District</b>
Sidewalk	109834				

### Temporary Operation

<b>WO Created Date</b>	<b>Schedule Start</b>	<b>Last Completed WO Activity</b>	<b>Last Completed WO Desc</b>
Feb 02, 2023		Boulevard Hazard	Boulevard Hazard

### JOB DETAILS

**Where:** 13 meters W of Pole # .5-10 (Pole .5-10 at NW corner of Smithe & Burrard)

**Hazard:** Sidewalk Obstacle

**Comments:** obstacle is pole sleeve with no pole in it.

This work order created by inspection # 239674

Inspector **s.22(1)** (RUPERT GUFFEY)

Inspected Date: 2023-02-02 8:19:25 AM

Feb 3/23 - Placed cone and 36 on pole sleeve with no pole. Refer to Sign Shop - Crew 67

Work Completed

Feb 09/23 [OT] Paperwork forwarded to TO Office for Supervisor review.

Feb 10/23 [OT] Assigned to ES, RS, MR / E1107 as per TO Supervisor.

Feb 10/23 [ES] Empty Meter Sleeve. Installed Pipe 10' and made safe.

- -OTHER WORK REQUIRED Sleeves need to be removed.

Feb 13/23 [MS] Traffic Ops Crews have identified this Sleeve as a Meter Sleeve. Issue to be forwarded to Parking Ops for review.

**Asset Comments:**

# TEOD Work Slip

Assigned By	Assigned Date	Assigned Time	Arrival Date	Arrival Time
DANNY GURNIAK	2023/02/03	13:56		
<b>PROBLEM FOUND</b>				
<hr/> <hr/>				
<b>WORK COMPLETED</b>				
<hr/> <hr/> <hr/> <hr/> <hr/> <hr/> <hr/>				
Functional Test		TCP/LCT		Stray Voltage Check
<b>OTHER WORK REQUIRED</b>				
<hr/> <hr/> <hr/>				
<b>ADDITIONAL COMMENTS</b>				
<hr/> <hr/> <hr/>				
Completed Date	Completed Time	Crew Member Name	Crew Member Name	Crew Member Name
Feb 13, 2023	00:00	MARK ANSTICE		
<b>Print Name</b>		<b>Signature</b>		<b>Truck #</b>
				RMS67



# Work Order Summary

**Work Order #:** 1349938      **T\_F1\_TO-21-7366**      **Form 1 Work Request**  
**Asset ID #:**  
**Group Project #:** Traffic Operations  
**Location:** 900 - 999 SMITHE ST  
**Operational Status:**      **Ref #1: :**      **Ref #2: T\_Form1: 223217**  
**Service Request:**      **Contact Name:**      **Phone#:**  
**Requested By:** Eng Projects Branch      **Requested Date:** Oct 25, 2021      **Initiated Date:** Oct 26, 2021

**Assigned By:** BHAPINDER SANDHU      **Assigned To:**  
**Assigned Date:** Nov 02, 2021      **Crew ID:**      **Work Started:** Nov 02, 2021  
**Result:** Completed      **Work Completed:** Oct 05, 2022  
**Comments:**

Smithe St Bikeway 2021 - 900-1000 Smithe St / 1000-1100 Haro St  
 Requested by Serena Sun / Projects (604.871.6596) in FORM 1 #DOC/2021/223217 and DWG #DOC/2021/248022  
 Bi-directional protected bike lane on the north side of Smithe St from Richards to Haro. Include protected intersection at Hornby and Thurlow intersection (connection to Haro Bikeway). Exclude most scope at Richards/Smithe (part of Richards Bikeway). Signal modifications for protected-only rights and contra-flow bikes.  
 Upgrade Smithe Street for all ages and abilities cycling in both directions  
 Improve safety by addressing conflicts with through and turning drivers  
 Design for cycling connections to build a AAA network  
 Maintain people-moving capacity  
 Balance parking with active transportation upgrades

**Work Completed**  
 Nov 02/21 ☐ Repremarked for 5x Sleeves.  
 Nov 04/21 ☐ Repremarked. All Pipes installed on Island except where Island was not completed.  
 Nov 08/21 ☐ Installed Pipe and Signs as per DWG.  
 Nov 13/21 ☐ Installed all Signs except for Granville to Lane West. Pipes need to be cut and cemented on 700, 800, 1000. Signs not done on 1000 except for North Side.  
 Nov 15/21 ☐ Eradication on North Side of Smithe - East of Burrard.  
 Nov 16/21 ☐ Finished Eradication - Installed CL Turn Bays - Temp Tape. Layout and installed 2x Arrows - 3m Torchdown.  
 Nov 17/21 ☐ Eradication for Burrard to Thurlow St.  
 Nov 18/21 ☐ Eradication; Installed Signs and Pipes as per DWG.  
 Nov 19/21 ☐ Installed Signs, Pipes, Piers. Some Sleeves are Piers. Need BCOne Call.  
 Dec 04/21 ☐ Installed Pipes, Piers, Signs, Bandit and removed Sleeves.  
 Dec 14/21 ☐ Removed all Sleeves and Pipes.  
 Dec 20/21 [RP] Checked Smithe St at the Lane EAST of Seymour St, WEST Side 900 Hornby St, 1000 Haro St. 1x Pier, relocate Pipe 10' and NSAT Sign. Removed Sleeve.  
 Dec 21/21 [D1657] Layout and install Bike Lane on the SWC. Installed 2x Straight Arrows. Temp Tape on 2x Right Turn Arrows. WEST Side Hornby St and 1000 Haro St COMPLETED.  
 Feb 03/22 ☐ Completed install / removed Signs and Pipes along Haro and Thurlow.  
 Oct 05/22 ☐ Completed as per TO Supervisors.

## Work Orders & Accomplishments:

WO#	Result	Completed Date	
1349938	Completed	Oct 05, 2022	

## TEOBSN:

Date	Location	Item Code Description	Quantity	Truck#	Assigned To	Reason	Comments
	900-999 SMITHE ST						

**Work Area Prefix:** M      **SAP Network #:**      **WBS#:**  
**Parent Network:**      **Billable:** N

# Work Order Summary

Cost Summary	Estimated Costs	Actual Costs	Difference
Equipment	\$0.00	\$0.00	\$0.00
Labour	\$0.00	\$0.00	\$0.00
Material	\$0.00	\$0.00	\$0.00
Other	\$0.00	\$0.00	\$0.00
	\$0.00	\$0.00	\$0.00
<b>TOTAL</b>	<u>\$0.00</u>	<u>\$0.00</u>	<u>\$0.00</u>



# Work Order Summary

**Work Order #:** 1661762      Boulevard Hazard  
**Asset ID #:** 109834  
**Group Project #:** Streets Operations  
**Location:** 1000 - 1099 SMITHE ST  
**Operational Status:**      **Ref #1:** Standing: EER1154468      **Ref #2:**  
**Service Request:**      **Contact Name:**      **Phone#:**  
**Requested By:**      **Requested Date:**      **Initiated Date:** Feb 02, 2023

**Assigned By:** DANNY GURNIAK      **Assigned To:** MARK ANSTICE  
**Assigned Date:** Feb 03, 2023      **Crew ID:** RMS67      **Work Started:**  
**Result:** WorkRefer      **Work Completed:** Feb 13, 2023

**Comments:** Where: 13 meters W of Pole # .5-10 (Pole .5-10 at NW corner of Smithe & Burrard)  
 Hazard: Sidewalk Obstacle  
 Comments: obstacle is pole sleeve with no pole in it.  
  
 This work order created by inspection # 239674  
 Inspector s.22(1) RUPERT GUFFEY)  
 Inspected Date: 2023-02-02 8:19:25 AM  
  
 Feb 3/23 - Placed cone and 36 on pole sleeve with no pole. Refer to Sign Shop - Crew 67  
  
 Work Completed  
 Feb 09/23 [OT] Paperwork forwarded to TO Office for Supervisor review.  
 Feb 10/23 [OT] Assigned to ES, RS, MR / E1107 as per TO Supervisor.  
 Feb 10/23 [ES] Empty Meter Sleeve. Installed Pipe 10' and made safe.  
 OTHER WORK REQUIRED Sleeves need to be removed.  
 Feb 13/23 [MS] Traffic Ops Crews have identified this Sleeve as a Meter Sleeve. Issue to be forwarded to Parking Ops for review.

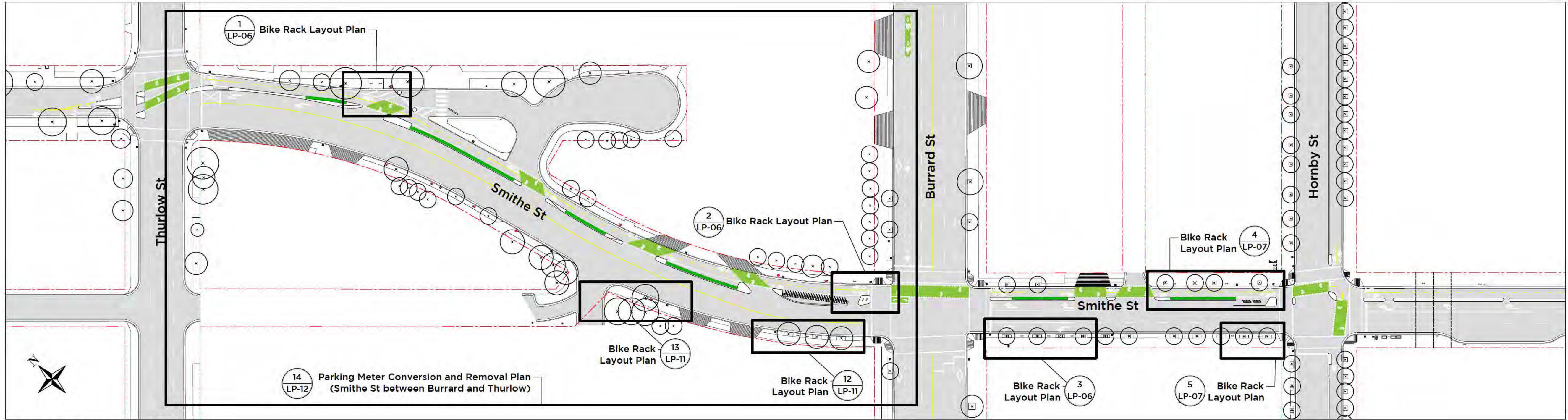
## Work Orders & Accomplishments:

WO#	Result	Completed Date	
1661762	WorkRefer	Feb 13, 2023	

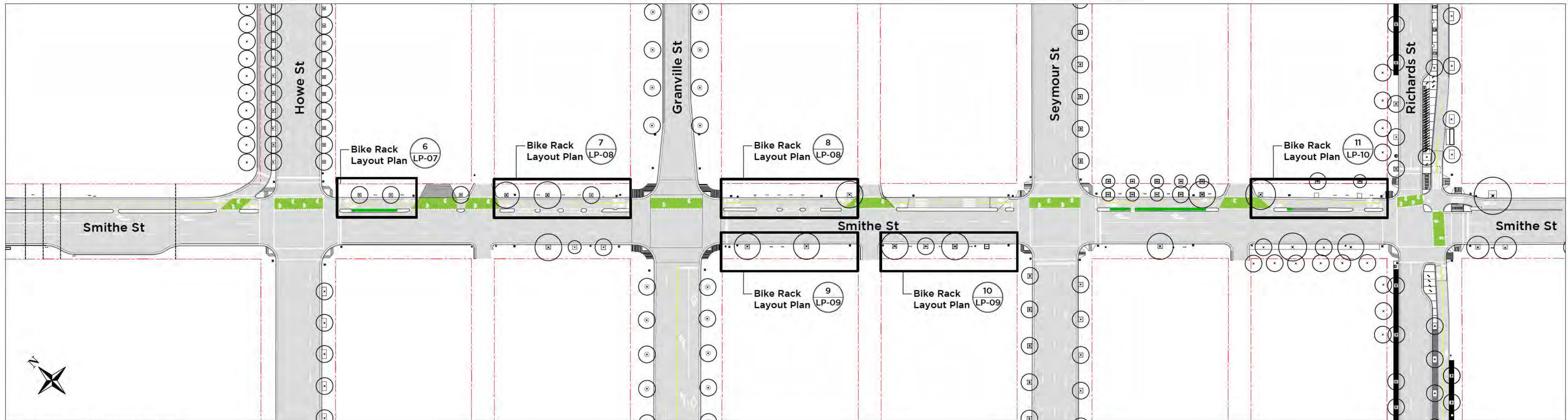
**Work Area Prefix:** R      **SAP Network #:**      **WBS#:**  
**Parent Network:**      **Billable:** N

Cost Summary	Estimated Costs	Actual Costs	Difference
Equipment	\$0.00	\$0.00	\$0.00
Labour	\$0.00	\$0.00	\$0.00
Material	\$0.00	\$0.00	\$0.00
Other	\$0.00	\$0.00	\$0.00
	\$0.00	\$0.00	\$0.00
<b>TOTAL</b>	<u>\$0.00</u>	<u>\$0.00</u>	<u>\$0.00</u>





THURLOW ST. to HORNBY ST.



HOWE ST. to RICHARDS ST.

NOTES:

CONCRETE NETWORK NUMBERS

ITEM	NETWORK
CURB - STANDARD STRAIGHT	
CURB - STANDARD CURVED	
CURB - HEAVY STRAIGHT	
CURB - HEAVY CURVED	
SIDEWALK - BROOM FINISHED	
SIDEWALK - EXPOSED AGGREGATE	
DRIVEWAY - RESIDENTIAL	
DRIVEWAY - COMMERCIAL	
MEDIAN - POUR IN PLACE	
MEDIAN - PLANTED	
BUS SLAB	

ASPHALT NETWORK NUMBERS

ITEM	NETWORK
PAVING - GRIND & OVERLAY RESIDENTIAL	
PAVING - GRIND & OVERLAY COMMERCIAL	
PAVING - ARTERIAL PAVEMENT	
CUT OUT & PATCH - RESIDENTIAL	
CUT OUT & PATCH - COMMERCIAL	
RAISED CROSSWALK	
ASPHALT PATH	

MISCELLANEOUS NETWORK NUMBERS

ITEM	NETWORK
BOULEVARD - SOIL & SOD	
LANDSCAPING - TREES & PLANTING	
TRAFFIC CONTROL - CONCRETE	
TRAFFIC CONTROL - ASPHALT	
SITE SAFETY	

REFERENCE DRAWINGS

TYPE	PLAN NUMBER
TRANSPORTATION DESIGN	

ATTENTION

THE CITY OF VANCOUVER ASSUMES NO RESPONSIBILITY FOR THE CORRECTNESS OF THE INFORMATION SHOWN.  
ALL DIMENSIONS SHOWN ON THIS PLAN ARE AS DESIGNED AND NOT AS CONSTRUCTED, AND SHOULD BE FIELD CHECKED BY APPLICANT OR THEIR AGENT.  
ELEVATIONS & COORDINATES SHOWN ON THIS PLAN ARE IN METRES BASED ON G.V.R.D. NAD83 DATUM (ISSUED MARCH 31, 2005).

LEGEND



NO.	DATE	REVISION	BY	CHK
1	08-18-2021	ISSUED FOR CONSTRUCTION	AM	
2	01-12-2022	ISSUED FOR CONSTRUCTION	AF	
3				
4				
5				
6				
7				
8				
9				

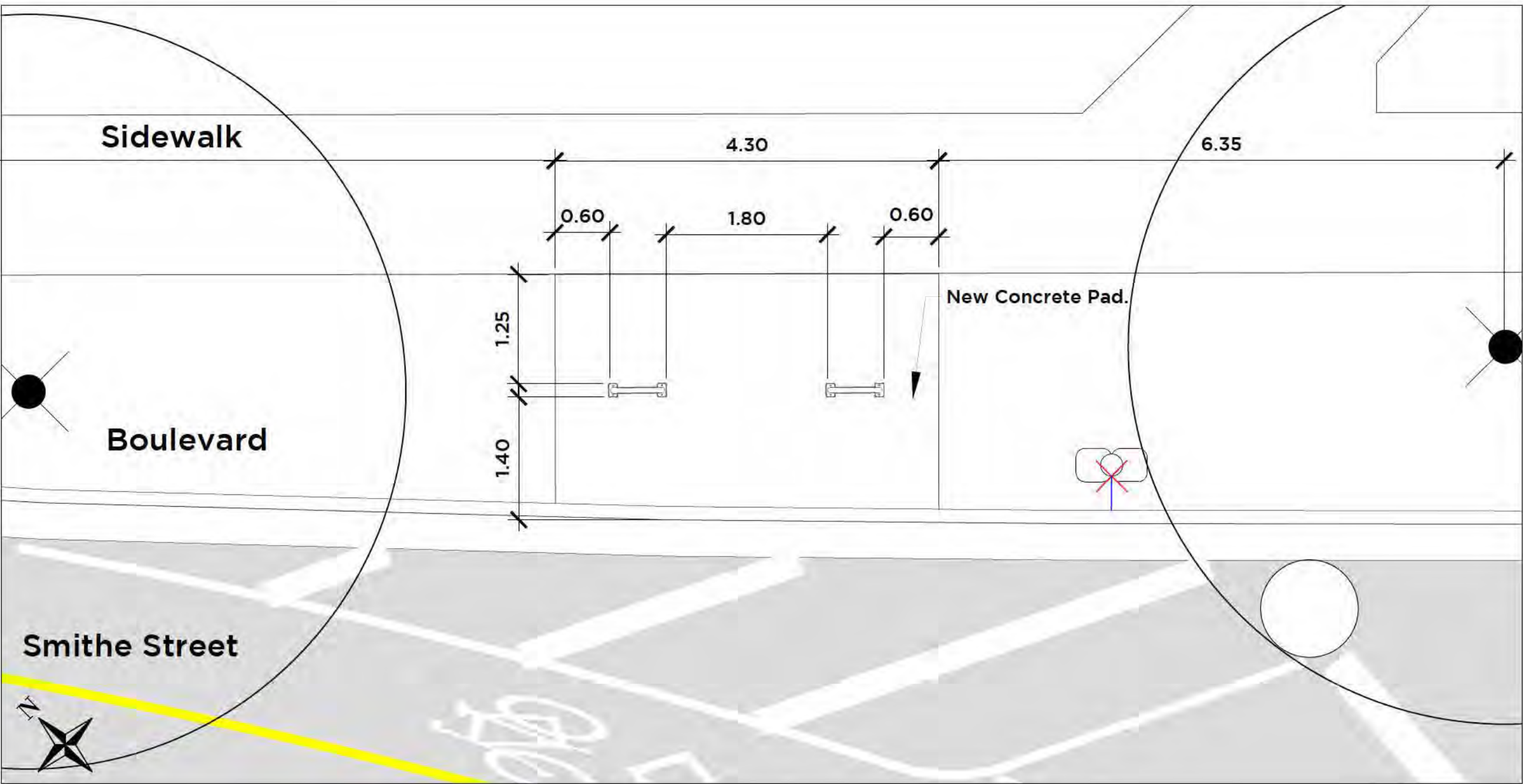
CITY OF VANCOUVER - ENGINEERING SERVICES

DIV./BR. TRANSPORTATION DESIGN BRANCH	
DATE:	DESIGN: AM
DWG: AF	CHK:
REFS: 000000	

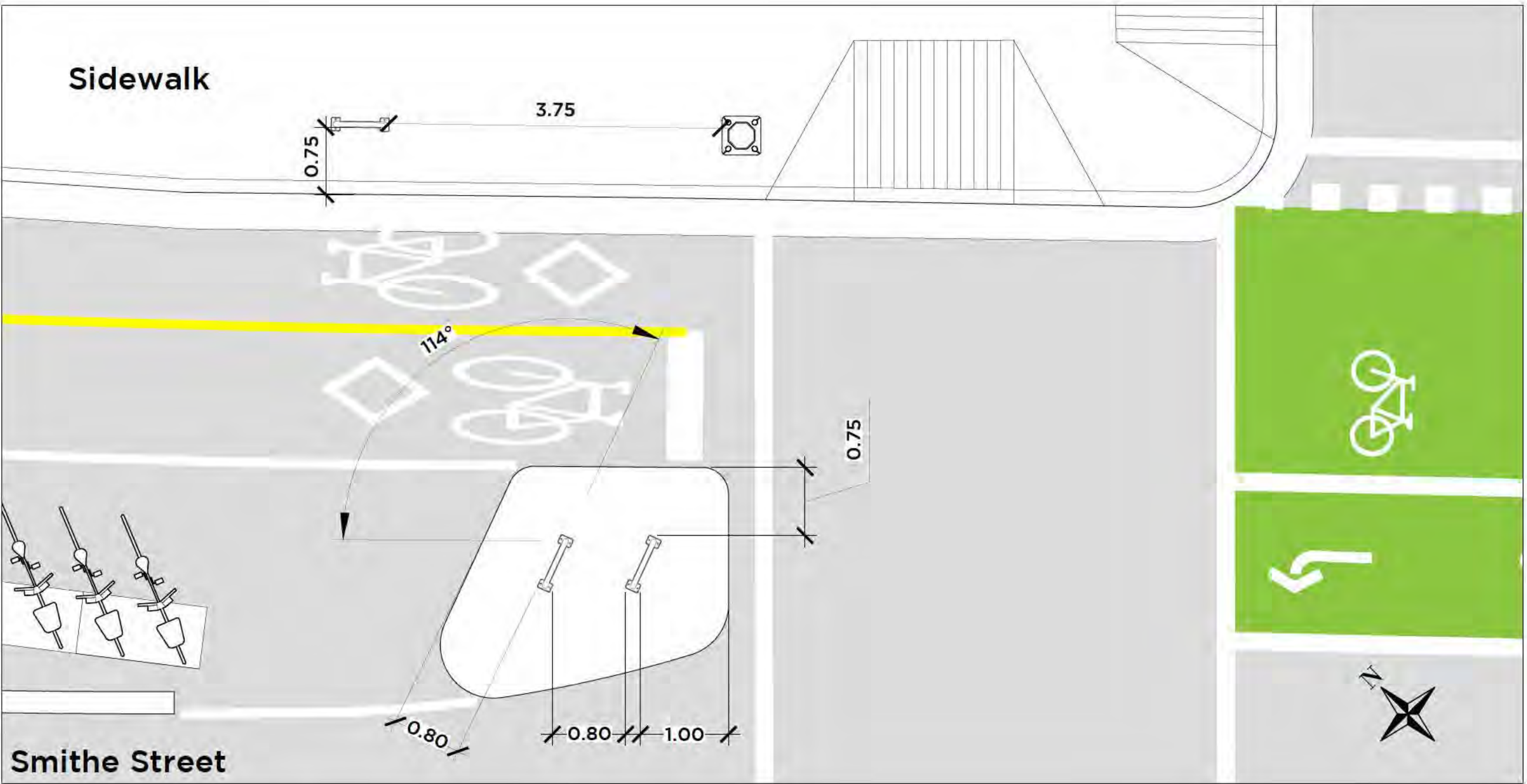
SMITHE ST BIKEWAY 2021  
THURLOW TO RICHARDS  
LANDSCAPE PLANS  
LANDSCAPE FURNITURE - KEY

SCALE: 1:600
DIRECTORY:
DWG. NO. 2018-32-E-LF-01
SHEET...1...OF 6... REVISION: .....

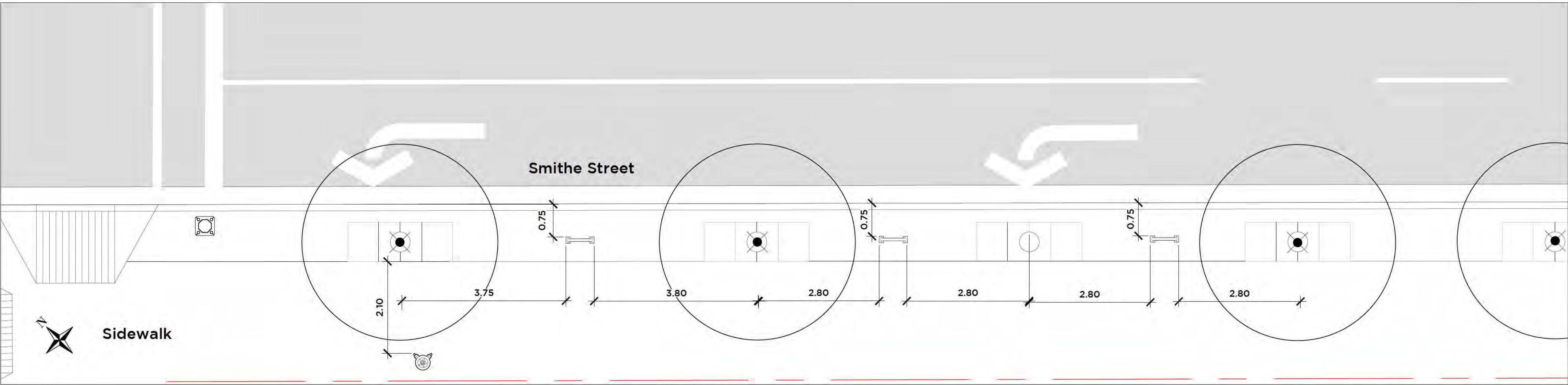




1 Bike Rack Layout  
LP-06 Scale: 1:50



2 Bike Rack Layout  
LP-06 Scale: 1:50



3 Bike Rack Layout  
LP-06 Scale: 1:50

**STREET FURNITURE NOTES**  
- Bike Rack Quantities  
-- 33: CofV Standard Inverted U Rack Bike Racks, Colour: Standard  
-- 1: Parking Meter converted into Bike Rack

NOTES:

CONCRETE NETWORK NUMBERS

ITEM	NETWORK
CURB – STANDARD STRAIGHT	
CURB – STANDARD CURVED	
CURB – HEAVY STRAIGHT	
CURB – HEAVY CURVED	
SIDEWALK – BROOM FINISHED	
SIDEWALK – EXPOSED AGGREGATE	
DRIVEWAY – RESIDENTIAL	
DRIVEWAY – COMMERCIAL	
MEDIAN – POUR IN PLACE	
MEDIAN – PLANTED	
BUS SLAB	

ASPHALT NETWORK NUMBERS

ITEM	NETWORK
PAVING – GRIND & OVERLAY RESIDENTIAL	
PAVING – GRIND & OVERLAY COMMERCIAL	
PAVING – ARTERIAL PAVEMENT	
CUT OUT & PATCH – RESIDENTIAL	
CUT OUT & PATCH – COMMERCIAL	
RAISED CROSSWALK	
ASPHALT PATH	

MISCELLANEOUS NETWORK NUMBERS

ITEM	NETWORK
BOULEVARD – SOIL & SOD	
LANDSCAPING – TREES & PLANTING	
TRAFFIC CONTROL – CONCRETE	
TRAFFIC CONTROL – ASPHALT	
SITE SAFETY	

REFERENCE DRAWINGS

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ELEVATIONS & COORDINATES SHOWN ON THIS PLAN ARE IN METRES BASED ON G.V.R.D. NAD83 DATUM (ISSUED MARCH 31, 2005).

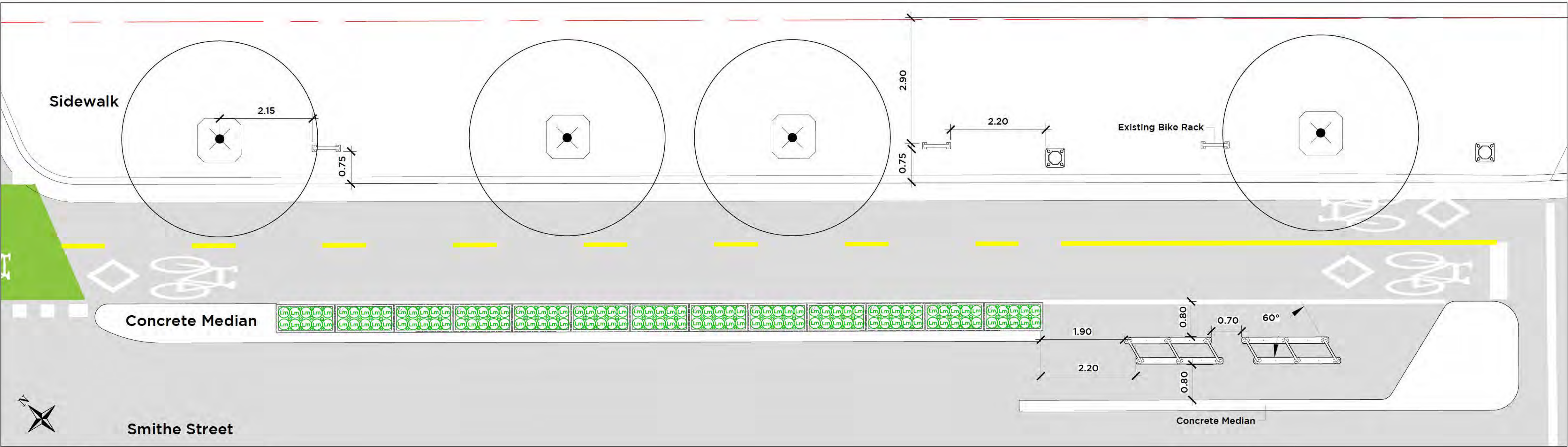


LEGEND

NO.	DATE	REVISION	BY	CHK	CITY OF VANCOUVER – ENGINEERING SERVICES			
1	08-18-2021	ISSUED FOR CONSTRUCTION	AM		DIV./BR. TRANSPORTATION DESIGN BRANCH			
2	01-12-2022	ISSUED FOR CONSTRUCTION	AF					
3					DATE: DESIGN: AM			
4								
5					DWG: AF CHK:			
6								
7					REFS: 000000			
8								
9								

SMITHE ST BIKEWAY 2021 THURLOW TO RICHARDS LANDSCAPE PLANS LANDSCAPE FURNITURE		SCALE: As Noted
		DIRECTORY:
		DWG. NO. 2018-32-E-LF-02
		SHEET...2...OF 6... REVISION:.....



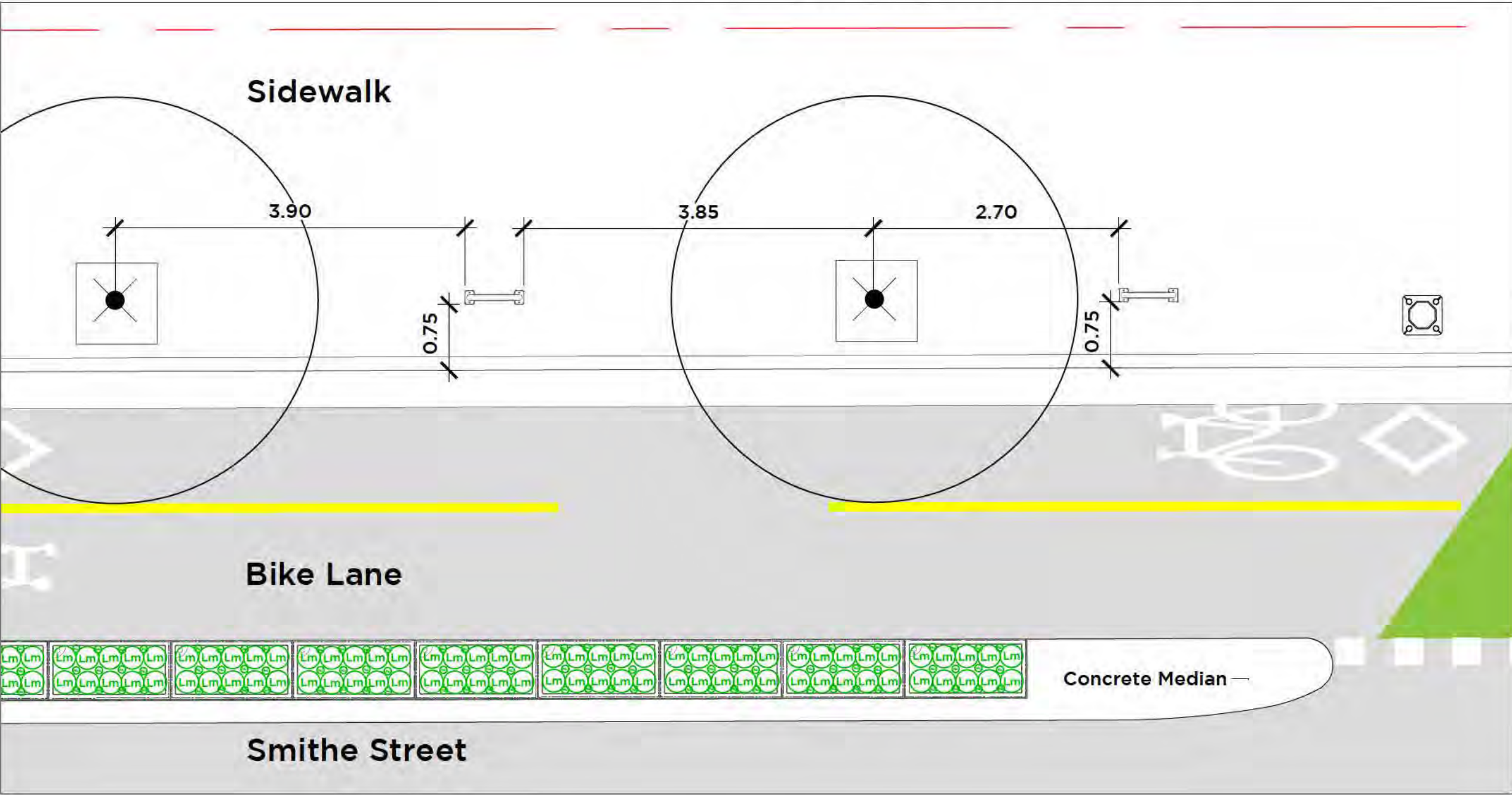


4 Bike Rack Layout  
LP-07 Scale: 1:50

- STREET FURNITURE NOTES
- Bike Rack Quantities
  - 33: CofV Standard Inverted U Rack Bike Racks, Colour: Standard
  - 1: Parking Meter converted into Bike Rack



5 Bike Rack Layout  
LP-07 Scale: 1:50



6 Bike Rack Layout  
LP-07 Scale: 1:50

NOTES:

CONCRETE NETWORK NUMBERS	
ITEM	NETWORK
CURB – STANDARD STRAIGHT	
CURB – STANDARD CURVED	
CURB – HEAVY STRAIGHT	
CURB – HEAVY CURVED	
SIDEWALK – BROOM FINISHED	
SIDEWALK – EXPOSED AGGREGATE	
DRIVEWAY – RESIDENTIAL	
DRIVEWAY – COMMERCIAL	
MEDIAN – POUR IN PLACE	
MEDIAN – PLANTED	
BUS SLAB	

ASPHALT NETWORK NUMBERS	
ITEM	NETWORK
PAVING – GRIND & OVERLAY RESIDENTIAL	
PAVING – GRIND & OVERLAY COMMERCIAL	
PAVING – ARTERIAL PAVEMENT	
CUT OUT & PATCH – RESIDENTIAL	
CUT OUT & PATCH – COMMERCIAL	
RAISED CROSSWALK	
ASPHALT PATH	

MISCELLANEOUS NETWORK NUMBERS	
ITEM	NETWORK
BOULEVARD – SOIL & SOD	
LANDSCAPING – TREES & PLANTING	
TRAFFIC CONTROL – CONCRETE	
TRAFFIC CONTROL – ASPHALT	
SITE SAFETY	

REFERENCE DRAWINGS	
TYPE	PLAN NUMBER
TRANSPORTATION DESIGN	

ATTENTION

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ELEVATIONS & COORDINATES SHOWN ON THIS PLAN ARE IN METRES BASED ON G.V.R.D. NAD83 DATUM (ISSUED MARCH 31, 2005).



LEGEND

NO.	DATE	REVISION	BY	CHK
1	08-18-2021	ISSUED FOR CONSTRUCTION	AM	
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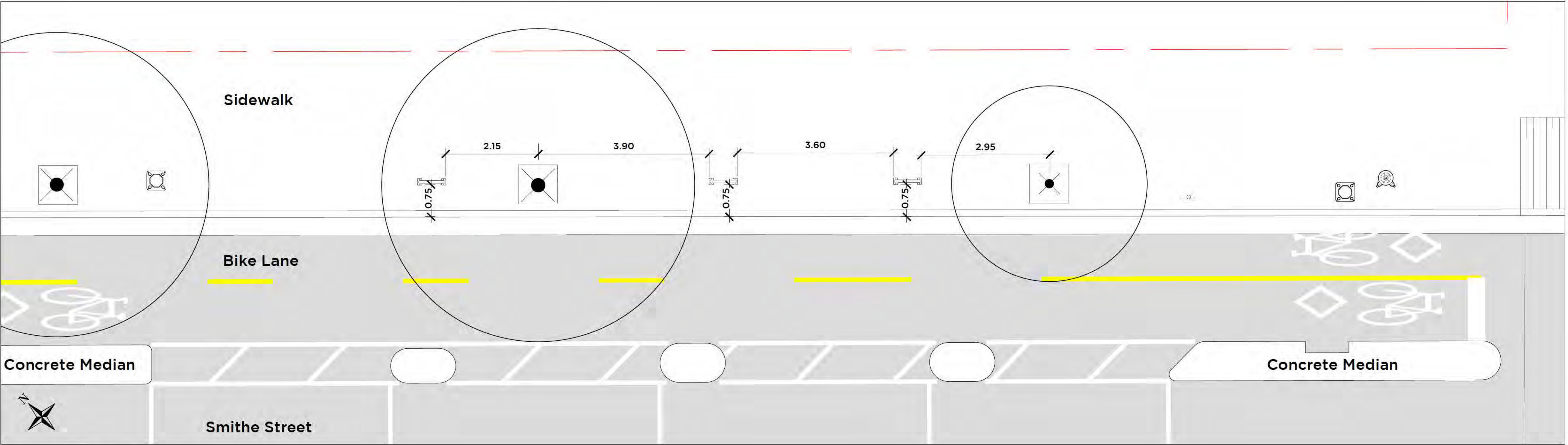
CITY OF VANCOUVER – ENGINEERING SERVICES

DIV./BR. TRANSPORTATION DESIGN BRANCH	
DATE:	DESIGN: AM
DWG: AF	CHK:
REFS: 000000	

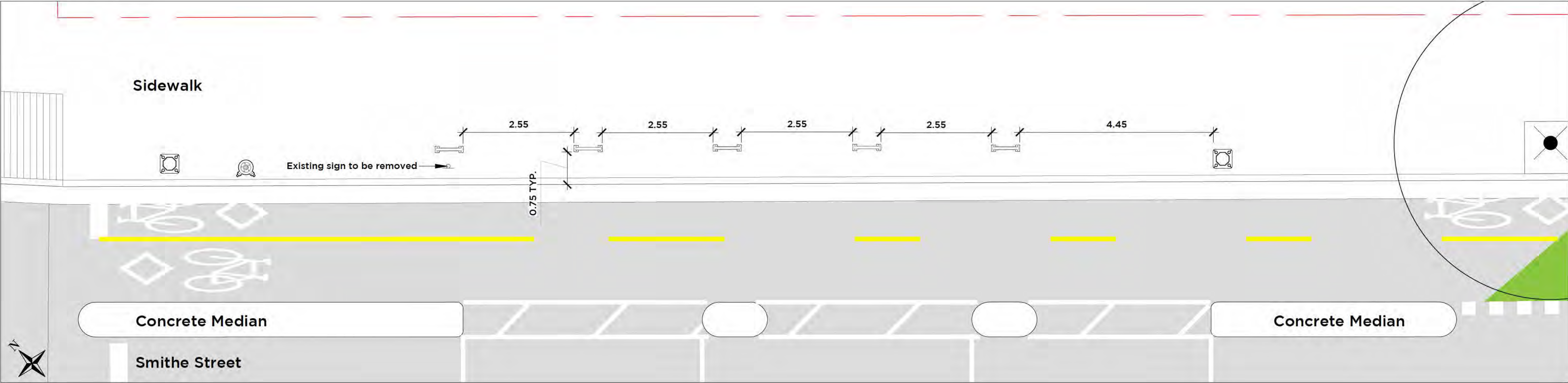
SMITHE ST BIKEWAY 2021  
THURLOW TO RICHARDS  
LANDSCAPE PLANS  
LANDSCAPE FURNITURE

SCALE: As Noted	
DIRECTORY:	
DWG. NO. 2018-32-E-LF-03	
SHEET...3...OF 6...	REVISION:.....





7 **Bike Rack Layout**  
LP-08 Scale: 1:50



8 **Bike Rack Layout**  
LP-08 Scale: 1:50

**STREET FURNITURE NOTES**  
- **Bike Rack Quantities**  
-- **33: CofV Standard Inverted U Rack Bike Racks, Colour: Standard**  
-- **1: Parking Meter converted into Bike Rack**

NOTES:

CONCRETE NETWORK NUMBERS

ITEM	NETWORK
CURB – STANDARD STRAIGHT	
CURB – STANDARD CURVED	
CURB – HEAVY STRAIGHT	
CURB – HEAVY CURVED	
SIDEWALK – BROOM FINISHED	
SIDEWALK – EXPOSED AGGREGATE	
DRIVEWAY – RESIDENTIAL	
DRIVEWAY – COMMERCIAL	
MEDIAN – POUR IN PLACE	
MEDIAN – PLANTED	
BUS SLAB	

ASPHALT NETWORK NUMBERS

ITEM	NETWORK
PAVING – GRIND & OVERLAY RESIDENTIAL	
PAVING – GRIND & OVERLAY COMMERCIAL	
PAVING – ARTERIAL PAVEMENT	
CUT OUT & PATCH – RESIDENTIAL	
CUT OUT & PATCH – COMMERCIAL	
RAISED CROSSWALK	
ASPHALT PATH	

MISCELLANEOUS NETWORK NUMBERS

ITEM	NETWORK
BOULEVARD – SOIL & SOD	
LANDSCAPING – TREES & PLANTING	
TRAFFIC CONTROL – CONCRETE	
TRAFFIC CONTROL – ASPHALT	
SITE SAFETY	

REFERENCE DRAWINGS

TYPE	PLAN NUMBER
TRANSPORTATION DESIGN	

ATTENTION

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ALL DIMENSIONS SHOWN ON THIS PLAN ARE AS DESIGNED AND NOT AS CONSTRUCTED, AND SHOULD BE FIELD CHECKED BY APPLICANT OR THEIR AGENT.  
ELEVATIONS & COORDINATES SHOWN ON THIS PLAN ARE IN METRES BASED ON G.V.R.D. NAD83 DATUM (ISSUED MARCH 31, 2005).

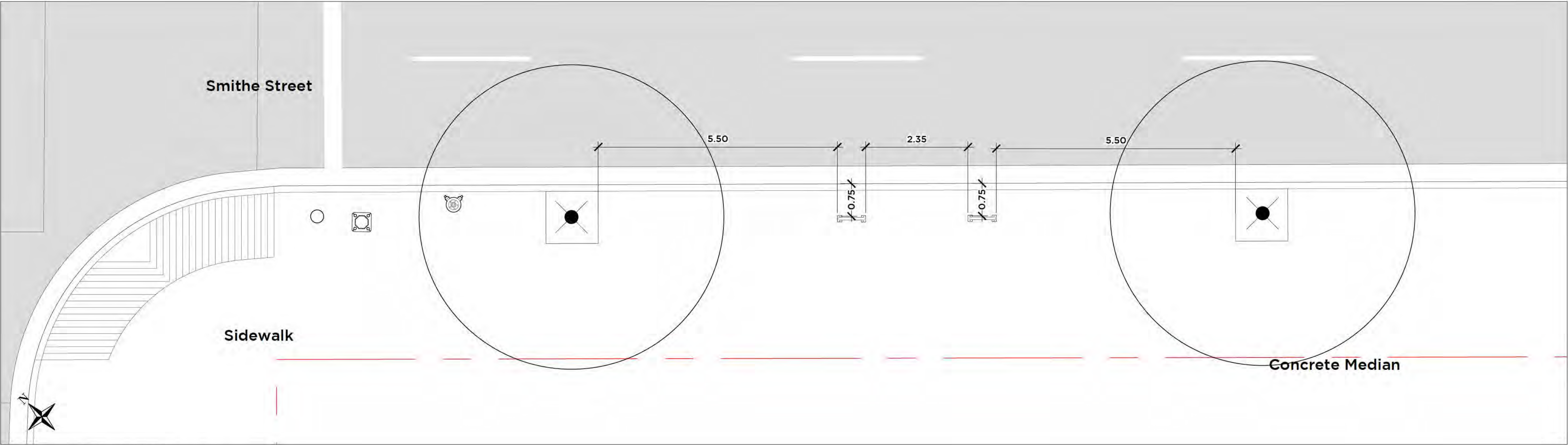


LEGEND

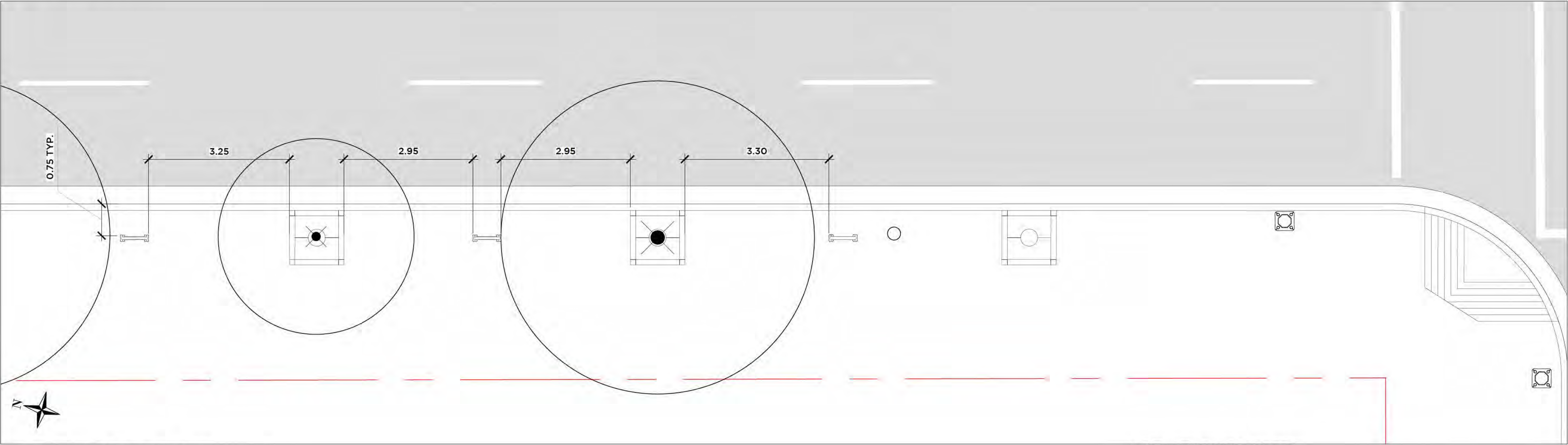
NO.	DATE	REVISION	BY	CHK
1	08-18-2021	ISSUED FOR CONSTRUCTION	AM	
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CITY OF VANCOUVER – ENGINEERING SERVICES			
DIV./BR.	TRANSPORTATION DESIGN BRANCH		
DATE:	DESIGN: AM		
DWG: AF	CHK:		
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SMITHE ST BIKEWAY 2021 THURLOW TO RICHARDS LANDSCAPE PLANS LANDSCAPE FURNITURE		SCALE: As Noted	DIRECTORY:
		DWG. NO. 2018-32-E-LF-04	
		SHEET 4 OF 6	REVISION: .....





9 Bike Rack Layout  
LP-09 Scale: 1:50



10 Bike Rack Layout  
LP-09 Scale: 1:50

NOTES:

CONCRETE NETWORK NUMBERS	
ITEM	NETWORK
CURB – STANDARD STRAIGHT	
CURB – STANDARD CURVED	
CURB – HEAVY STRAIGHT	
CURB – HEAVY CURVED	
SIDEWALK – BROOM FINISHED	
SIDEWALK – EXPOSED AGGREGATE	
DRIVEWAY – RESIDENTIAL	
DRIVEWAY – COMMERCIAL	
MEDIAN – POUR IN PLACE	
MEDIAN – PLANTED	
BUS SLAB	

ASPHALT NETWORK NUMBERS	
ITEM	NETWORK
PAVING – GRIND & OVERLAY RESIDENTIAL	
PAVING – GRIND & OVERLAY COMMERCIAL	
PAVING – ARTERIAL PAVEMENT	
CUT OUT & PATCH – RESIDENTIAL	
CUT OUT & PATCH – COMMERCIAL	
RAISED CROSSWALK	
ASPHALT PATH	

MISCELLANEOUS NETWORK NUMBERS	
ITEM	NETWORK
BOULEVARD – SOIL & SOD	
LANDSCAPING – TREES & PLANTING	
TRAFFIC CONTROL – CONCRETE	
TRAFFIC CONTROL – ASPHALT	
SITE SAFETY	

REFERENCE DRAWINGS	
TYPE	PLAN NUMBER
TRANSPORTATION DESIGN	

ATTENTION

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ELEVATIONS & COORDINATES SHOWN ON THIS PLAN ARE IN METRES BASED ON G.V.R.D. NAD83 DATUM (ISSUED MARCH 31, 2005).

STREET FURNITURE NOTES

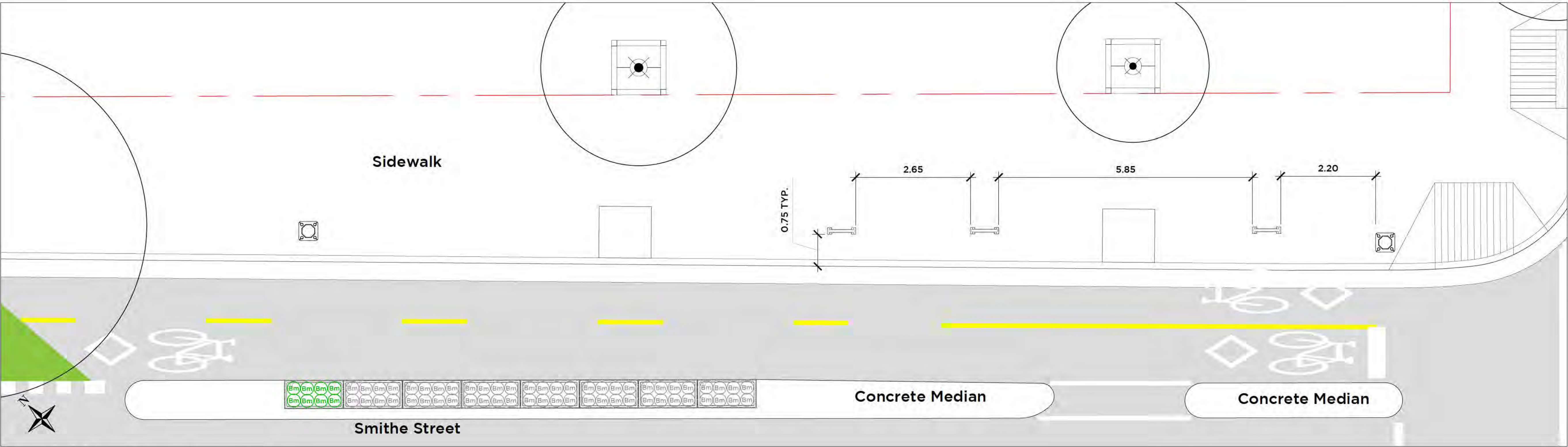
- Bike Rack Quantities
- 33: CofV Standard Inverted U Rack Bike Racks, Colour: Standard
- 1: Parking Meter converted into Bike Rack



LEGEND

NO.		DATE	REVISION	BY	CHK	CITY OF VANCOUVER – ENGINEERING SERVICES		SMITHE ST BIKEWAY 2021 THURLOW TO RICHARDS LANDSCAPE PLANS LANDSCAPE FURNITURE		SCALE: As Noted	
1	08-18-2021	ISSUED FOR CONSTRUCTION		AM		DIV./BR. TRANSPORTATION DESIGN BRANCH	DATE: DESIGN: AM	DWG: AF	CHK:	DWG. NO. 2018-32-E-LF-05	SHEET: 5 OF 6
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11  
LP-10

## Bike Rack Layout

Scale: 1:50

- STREET FURNITURE NOTES**
- **Bike Rack Quantities**
  - **33: CofV Standard Inverted U Rack Bike Racks, Colour: Standard**
  - **1: Parking Meter converted into Bike Rack**

NOTES:

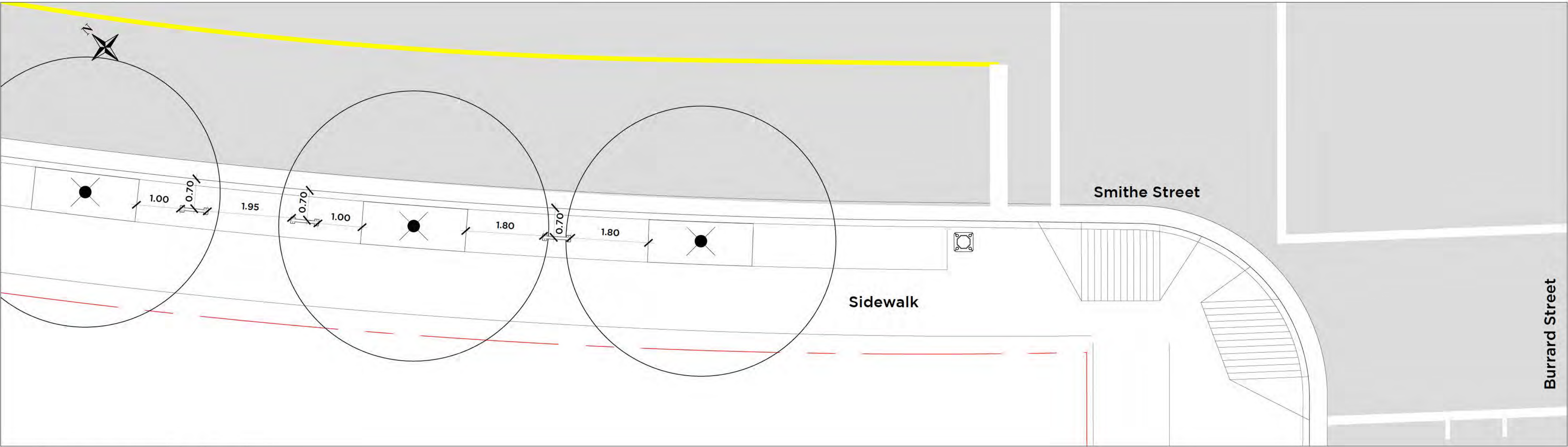
CONCRETE NETWORK NUMBERS	
ITEM	NETWORK
CURB – STANDARD STRAIGHT	
CURB – STANDARD CURVED	
CURB – HEAVY STRAIGHT	
CURB – HEAVY CURVED	
SIDEWALK – BROOM FINISHED	
SIDEWALK – EXPOSED AGGREGATE	
DRIVEWAY – RESIDENTIAL	
DRIVEWAY – COMMERCIAL	
MEDIAN – POUR IN PLACE	
MEDIAN – PLANTED	
BUS SLAB	
ASPHALT NETWORK NUMBERS	
ITEM	NETWORK
PAVING – GRIND & OVERLAY RESIDENTIAL	
PAVING – GRIND & OVERLAY COMMERCIAL	
PAVING – ARTERIAL PAVEMENT	
CUT OUT & PATCH – RESIDENTIAL	
CUT OUT & PATCH – COMMERCIAL	
RAISED CROSSWALK	
ASPHALT PATH	
MISCELLANEOUS NETWORK NUMBERS	
ITEM	NETWORK
BOULEVARD – SOIL & SOD	
LANDSCAPING – TREES & PLANTING	
TRAFFIC CONTROL – CONCRETE	
TRAFFIC CONTROL – ASPHALT	
SITE SAFETY	
REFERENCE DRAWINGS	
TYPE	PLAN NUMBER
TRANSPORTATION DESIGN	
ATTENTION	
THE CITY OF VANCOUVER ASSUMES NO RESPONSIBILITY FOR THE CORRECTNESS OF THE INFORMATION SHOWN.	
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ELEVATIONS & COORDINATES SHOWN ON THIS PLAN ARE IN METRES BASED ON G.V.R.D. NAD83 DATUM (ISSUED MARCH 31, 2005).	



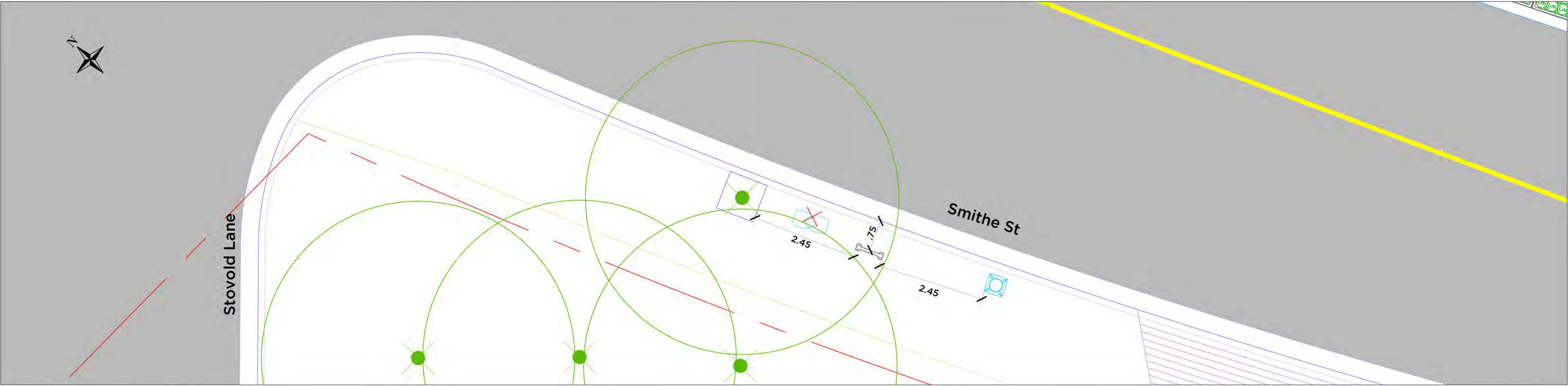
LEGEND

NO.	DATE	REVISION	BY	CHK	CITY OF VANCOUVER – ENGINEERING SERVICES		
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2	01-12-2022	ISSUED FOR CONSTRUCTION	AF				
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12 **Bike Rack Layout**  
LP-11 Scale: 1:50



13 **Bike Rack Layout**  
LP-11 Scale: 1:50

NOTES:

CONCRETE NETWORK NUMBERS

ITEM	NETWORK
CURB – STANDARD STRAIGHT	
CURB – STANDARD CURVED	
CURB – HEAVY STRAIGHT	
CURB – HEAVY CURVED	
SIDEWALK – BROOM FINISHED	
SIDEWALK – EXPOSED AGGREGATE	
DRIVEWAY – RESIDENTIAL	
DRIVEWAY – COMMERCIAL	
MEDIAN – POUR IN PLACE	
MEDIAN – PLANTED	
BUS SLAB	

ASPHALT NETWORK NUMBERS

ITEM	NETWORK
PAVING – GRIND & OVERLAY RESIDENTIAL	
PAVING – GRIND & OVERLAY COMMERCIAL	
PAVING – ARTERIAL PAVEMENT	
CUT OUT & PATCH – RESIDENTIAL	
CUT OUT & PATCH – COMMERCIAL	
RAISED CROSSWALK	
ASPHALT PATH	

MISCELLANEOUS NETWORK NUMBERS

ITEM	NETWORK
BOULEVARD – SOIL & SOD	
LANDSCAPING – TREES & PLANTING	
TRAFFIC CONTROL – CONCRETE	
TRAFFIC CONTROL – ASPHALT	
SITE SAFETY	

REFERENCE DRAWINGS

TYPE	PLAN NUMBER
TRANSPORTATION DESIGN	

ATTENTION

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STREET FURNITURE NOTES

- Bike Rack Quantities
- 33: CofV Standard Inverted U Rack Bike Racks, Colour: Standard
- 1: Parking Meter converted into Bike Rack



LEGEND

NO.	DATE	REVISION	BY	CHK
1	08-18-2021	ISSUED FOR CONSTRUCTION	AM	
2	01-12-2022	ISSUED FOR CONSTRUCTION	AF	
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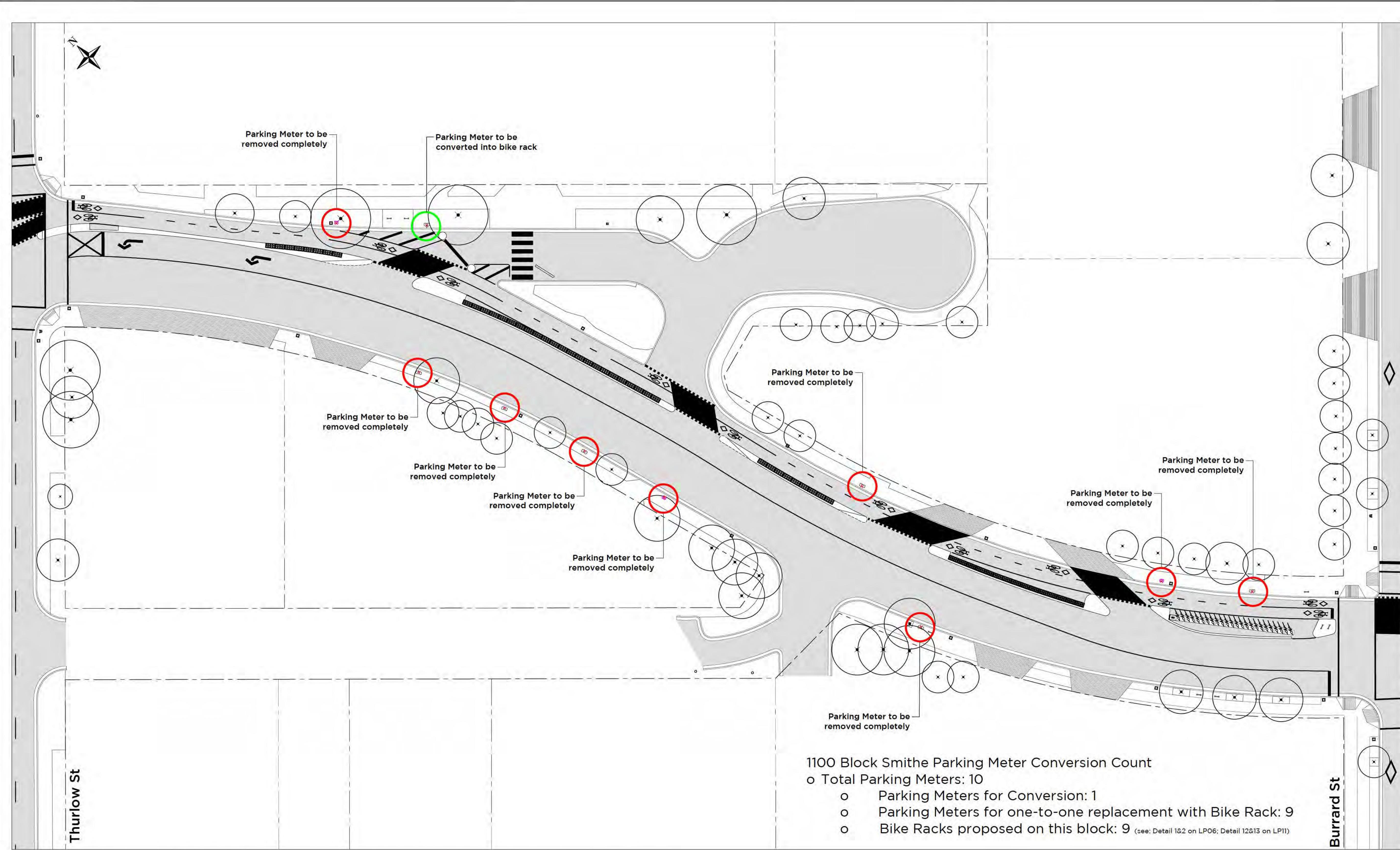
CITY OF VANCOUVER – ENGINEERING SERVICES

DIV./BR.	TRANSPORTATION DESIGN BRANCH
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SMITHE ST BIKEWAY 2021  
THURLOW TO RICHARDS  
LANDSCAPE PLANS  
LANDSCAPE FURNITURE

SCALE: As Noted
DIRECTORY:
DWG. NO. 2018-32-E-LF-06
SHEET...6...OF 6... REVISION:.....





NOTES:

CONCRETE NETWORK NUMBERS

ITEM	NETWORK
CURB – STANDARD STRAIGHT	
CURB – STANDARD CURVED	
CURB – HEAVY STRAIGHT	
CURB – HEAVY CURVED	
SIDEWALK – BROOM FINISHED	
SIDEWALK – EXPOSED AGGREGATE	
DRIVEWAY – RESIDENTIAL	
DRIVEWAY – COMMERCIAL	
MEDIAN – POUR IN PLACE	
MEDIAN – PLANTED	
BUS SLAB	

ASPHALT NETWORK NUMBERS

ITEM	NETWORK
PAVING – GRIND & OVERLAY RESIDENTIAL	
PAVING – GRIND & OVERLAY COMMERCIAL	
PAVING – ARTERIAL PAVEMENT	
CUT OUT & PATCH – RESIDENTIAL	
CUT OUT & PATCH – COMMERCIAL	
RAISED CROSSWALK	
ASPHALT PATH	

MISCELLANEOUS NETWORK NUMBERS

ITEM	NETWORK
BOULEVARD – SOIL & SOD	
LANDSCAPING – TREES & PLANTING	
TRAFFIC CONTROL – CONCRETE	
TRAFFIC CONTROL – ASPHALT	
SITE SAFETY	

REFERENCE DRAWINGS

TYPE	PLAN NUMBER
TRANSPORTATION DESIGN	

ATTENTION

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- 1100 Block Smithe Parking Meter Conversion Count
- o Total Parking Meters: 10
  - o Parking Meters for Conversion: 1
  - o Parking Meters for one-to-one replacement with Bike Rack: 9
  - o Bike Racks proposed on this block: 9 (see: Detail 1&2 on LP06; Detail 12&13 on LP11)

- STREET FURNITURE NOTES
- Bike Rack Quantities
  - 33: CofV Standard Inverted U Rack Bike Racks, Colour: Standard
  - 1: Parking Meter converted into Bike Rack

14  
LP-12  
Parking Meter Conversion and Removal Plan (Burrard - Thurlow)  
Scale: NTS



LEGEND

NO.	DATE	REVISION	BY	CHK
1	08-18-2021	ISSUED FOR CONSTRUCTION	AM	
2	01-12-2022	ISSUED FOR CONSTRUCTION	AF	
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CITY OF VANCOUVER – ENGINEERING SERVICES

DIV./BR.	TRANSPORTATION DESIGN BRANCH
DATE:	DESIGN: AM
DWG: AF	CHK:
REFS:	000000

SMITHE ST BIKEWAY 2021  
THURLOW TO RICHARDS  
LANDSCAPE PLANS  
LANDSCAPE FURNITURE

SCALE: As Noted
DIRECTORY:
DWG. NO. 2018-32-E-LF-06
SHEET: 6 OF 6
REVISION: .....



**From:** ["Pablo, Joshua" <Joshua.Pablo@vancouver.ca>](mailto:Joshua.Pablo@vancouver.ca)  
**To:** ["Reisig, Devin" <Devin.Reisig@vancouver.ca>](mailto:Devin.Reisig@vancouver.ca)  
["Mulhall, Ben" <Ben.Mulhall@vancouver.ca>](mailto:Ben.Mulhall@vancouver.ca)  
**CC:** ["Callow, Matthew" <Matthew.Callow@vancouver.ca>](mailto:Matthew.Callow@vancouver.ca)  
["Dong, Billy" <Billy.Dong@vancouver.ca>](mailto:Billy.Dong@vancouver.ca)  
["Thorseth, Leif" <Leif.Thorseth@vancouver.ca>](mailto:Leif.Thorseth@vancouver.ca)  
["Cheng, Alina" <Alina.Cheng@vancouver.ca>](mailto:Alina.Cheng@vancouver.ca)  
["Duggan, Ann" <Ann.Duggan@vancouver.ca>](mailto:Ann.Duggan@vancouver.ca)  
**Date:** 3/10/2023 11:38:51 AM  
**Subject:** RE: 2 Quick Questions re: Smithe

---

Hi Devin,

No, FMS had only converted a single parking meter into a post & ring rack. There are still plenty of parking meters that need to be cut and removed in the drawing set Ben attached previously.

Thank you,

Josh

---

**From:** Reisig, Devin <Devin.Reisig@vancouver.ca>  
**Sent:** Monday, October 17, 2022 2:04 PM  
**To:** Pablo, Joshua <Joshua.Pablo@vancouver.ca>; Mulhall, Ben <Ben.Mulhall@vancouver.ca>  
**Cc:** Callow, Matthew <Matthew.Callow@vancouver.ca>; Dong, Billy <Billy.Dong@vancouver.ca>; Thorseth, Leif <Leif.Thorseth@vancouver.ca>; Cheng, Alina <Alina.Cheng@vancouver.ca>; Duggan, Ann <Ann.Duggan@vancouver.ca>  
**Subject:** RE: 2 Quick Questions re: Smithe

Hi Josh,

Does that mean that the work needed by Parking Operations has been completed?

Devin

---

**From:** Pablo, Joshua <Joshua.Pablo@vancouver.ca>  
**Sent:** Monday, October 17, 2022 2:03 PM  
**To:** Mulhall, Ben <Ben.Mulhall@vancouver.ca>; Reisig, Devin <Devin.Reisig@vancouver.ca>  
**Cc:** Callow, Matthew <Matthew.Callow@vancouver.ca>; Dong, Billy <Billy.Dong@vancouver.ca>; Thorseth, Leif <Leif.Thorseth@vancouver.ca>; Cheng, Alina <Alina.Cheng@vancouver.ca>; Duggan, Ann <Ann.Duggan@vancouver.ca>  
**Subject:** RE: 2 Quick Questions re: Smithe

Hi Ben,

Yes, I've confirmed with FMS that the post & ring rack has been installed as per the drawing. They mentioned the post was missing from the base so they had it replaced.

Cheers,

**Josh Pablo, EIT** | (he, him)

Streets Furniture Coordinator | Street Activities

Engineering Services | City of Vancouver

[joshua.pablo@vancouver.ca](mailto:joshua.pablo@vancouver.ca)

C: 778-951-6326

---

**From:** Mulhall, Ben <[Ben.Mulhall@vancouver.ca](mailto:Ben.Mulhall@vancouver.ca)>

**Sent:** Monday, October 17, 2022 12:10 PM

**To:** Reisig, Devin <[Devin.Reisig@vancouver.ca](mailto:Devin.Reisig@vancouver.ca)>; Pablo, Joshua <[Joshua.Pablo@vancouver.ca](mailto:Joshua.Pablo@vancouver.ca)>

**Cc:** Callow, Matthew <[Matthew.Callow@vancouver.ca](mailto:Matthew.Callow@vancouver.ca)>; Dong, Billy <[Billy.Dong@vancouver.ca](mailto:Billy.Dong@vancouver.ca)>;

Thorseth, Leif <[Leif.Thorseth@vancouver.ca](mailto:Leif.Thorseth@vancouver.ca)>; Cheng, Alina <[Alina.Cheng@vancouver.ca](mailto:Alina.Cheng@vancouver.ca)>; Duggan,

Ann <[Ann.Duggan@vancouver.ca](mailto:Ann.Duggan@vancouver.ca)>

**Subject:** RE: 2 Quick Questions re: Smithe

Hi Devin,

Charges for this work should go to: **CERD90857**.

The meter posts to be removed are on Smithe, between Burrard and Thurlow. The last page of the attached drawing package shows the posts to be removed circled in red. Let me know if you need any more info.

One meter post is to be converted to a bike rack (circled in green on the attached drawing). @Josh, can you confirm if FMS converted that one?

Regards,

**Ben Mulhall**

Landscape Architect | Street Activities

---

**From:** Reisig, Devin <[Devin.Reisig@vancouver.ca](mailto:Devin.Reisig@vancouver.ca)>

**Sent:** Monday, October 17, 2022 11:25 AM

**To:** Mulhall, Ben <[Ben.Mulhall@vancouver.ca](mailto:Ben.Mulhall@vancouver.ca)>; Pablo, Joshua <[Joshua.Pablo@vancouver.ca](mailto:Joshua.Pablo@vancouver.ca)>

**Cc:** Callow, Matthew <[Matthew.Callow@vancouver.ca](mailto:Matthew.Callow@vancouver.ca)>; Dong, Billy <[Billy.Dong@vancouver.ca](mailto:Billy.Dong@vancouver.ca)>;

Thorseth, Leif <[Leif.Thorseth@vancouver.ca](mailto:Leif.Thorseth@vancouver.ca)>; Cheng, Alina <[Alina.Cheng@vancouver.ca](mailto:Alina.Cheng@vancouver.ca)>; Duggan,

Ann <[Ann.Duggan@vancouver.ca](mailto:Ann.Duggan@vancouver.ca)>

**Subject:** RE: 2 Quick Questions re: Smithe

Hi Ben,



I will need more info from you on these removals.

What block, what side, charge number etc.

Im at my desk if you want to just call me

**Devin Reisig**

(he/him)

Superintendent I

|

Parking Operations

City of Vancouver

[Devin.Reisig@Vancouver.ca](mailto:Devin.Reisig@Vancouver.ca)

Desk: 604-871-6041

Cell: 604-603-2362

---

**From:** Mulhall, Ben <[Ben.Mulhall@vancouver.ca](mailto:Ben.Mulhall@vancouver.ca)>

**Sent:** Monday, October 17, 2022 11:00 AM

**To:** Pablo, Joshua <[Joshua.Pablo@vancouver.ca](mailto:Joshua.Pablo@vancouver.ca)>; Reisig, Devin <[Devin.Reisig@vancouver.ca](mailto:Devin.Reisig@vancouver.ca)>

**Cc:** Callow, Matthew <[Matthew.Callow@vancouver.ca](mailto:Matthew.Callow@vancouver.ca)>; Dong, Billy <[Billy.Dong@vancouver.ca](mailto:Billy.Dong@vancouver.ca)>;

Thorseth, Leif <[Leif.Thorseth@vancouver.ca](mailto:Leif.Thorseth@vancouver.ca)>; Cheng, Alina <[Alina.Cheng@vancouver.ca](mailto:Alina.Cheng@vancouver.ca)>; Duggan,

Ann <[Ann.Duggan@vancouver.ca](mailto:Ann.Duggan@vancouver.ca)>

**Subject:** RE: 2 Quick Questions re: Smithe

Hi Josh,

Thanks for the update. It looks like all of PMB is away today, as is Leif who would normally coordinate the meter pole work with them....

@Devin, can you coordinate to have the meter poles on Smithe removed/replaced as required? Let me know if you need the design drawing package.

Regards,

**Ben Mulhall**

Landscape Architect | Street Activities

---

**From:** Pablo, Joshua <[Joshua.Pablo@vancouver.ca](mailto:Joshua.Pablo@vancouver.ca)>

**Sent:** Monday, October 17, 2022 9:45 AM

**To:** Mulhall, Ben <[Ben.Mulhall@vancouver.ca](mailto:Ben.Mulhall@vancouver.ca)>; Cheng, Alina <[Alina.Cheng@vancouver.ca](mailto:Alina.Cheng@vancouver.ca)>

**Cc:** Callow, Matthew <[Matthew.Callow@vancouver.ca](mailto:Matthew.Callow@vancouver.ca)>; Dong, Billy <[Billy.Dong@vancouver.ca](mailto:Billy.Dong@vancouver.ca)>;

Thorseth, Leif <[Leif.Thorseth@vancouver.ca](mailto:Leif.Thorseth@vancouver.ca)>

**Subject:** RE: 2 Quick Questions re: Smithe

Hi Everyone,

FMS has just let me know they have installed the Smithe St order of 33 U-Shaped racks / 2 Bike Corrals and 1 Post /ring rack, except for 2 locations noted below:

- ☐ Location 1/LP-06 – There was no concrete pad on location for 2 U-Shaped racks.
- ☐ Location 4/LP-07 – There is a flower pot planter in the single U-Shaped rack location.

Sorry for not being able to give an advanced notice. Would PMB be able to start the process now for removing the parking meter poles?

Let me know if you have any questions.

Regards,

**Josh Pablo, EIT** | (he, him)  
Streets Furniture Coordinator | Street Activities  
Engineering Services | City of Vancouver  
[joshua.pablo@vancouver.ca](mailto:joshua.pablo@vancouver.ca)  
C: 778-951-6326

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**From:** Pablo, Joshua  
**Sent:** Thursday, September 15, 2022 4:02 PM  
**To:** Mulhall, Ben <[Ben.Mulhall@vancouver.ca](mailto:Ben.Mulhall@vancouver.ca)>; Dong, Billy <[billy.dong@vancouver.ca](mailto:billy.dong@vancouver.ca)>; Thorseth, Leif <[leif.thorseth@vancouver.ca](mailto:leif.thorseth@vancouver.ca)>  
**Cc:** Callow, Matthew <[matthew.callow@vancouver.ca](mailto:matthew.callow@vancouver.ca)>  
**Subject:** RE: 2 Quick Questions re: Smithe

Hello,

Following up with this. FMS is caught up with the latest drawings and quantities as described by Ben below and will charge the work to the related SAP numbers as described by Chris below in the email chain.

Regards,

Josh

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**From:** Mulhall, Ben <[Ben.Mulhall@vancouver.ca](mailto:Ben.Mulhall@vancouver.ca)>  
**Sent:** Wednesday, September 14, 2022 11:24 AM  
**To:** Pablo, Joshua <[Joshua.Pablo@vancouver.ca](mailto:Joshua.Pablo@vancouver.ca)>; Dong, Billy <[Billy.Dong@vancouver.ca](mailto:Billy.Dong@vancouver.ca)>; Thorseth, Leif <[Leif.Thorseth@vancouver.ca](mailto:Leif.Thorseth@vancouver.ca)>  
**Cc:** Callow, Matthew <[Matthew.Callow@vancouver.ca](mailto:Matthew.Callow@vancouver.ca)>  
**Subject:** RE: 2 Quick Questions re: Smithe

Thanks for following up, Josh. Can you confirm if they will be manufacturing the other items per my



count from the drawings (July 18 email below)?

- ☐ Total stand-alone: 33 standard inverted U racks
- ☐ Plus: 6 inverted U racks on bike corrals (2 corrals)

To summarize next steps:

- ☐ FMS will notify Josh when they can schedule the rack installs (later this year)
- ☐ Josh to confirm they have the correct charge number for this work per Chris Darwent's email below
- ☐ Leif/Josh to give min. 1 week notice to PMB
- ☐ PMB to issue work order to P.Ops to remove meter posts shortly ahead of rack installs

Let me know if I've missed anything.

Cheers,

**Ben Mulhall**

Landscape Architect | Street Activities

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**From:** Pablo, Joshua <[Joshua.Pablo@vancouver.ca](mailto:Joshua.Pablo@vancouver.ca)>

**Sent:** Wednesday, September 14, 2022 11:05 AM

**To:** Dong, Billy <[Billy.Dong@vancouver.ca](mailto:Billy.Dong@vancouver.ca)>; Thorseth, Leif <[Leif.Thorseth@vancouver.ca](mailto:Leif.Thorseth@vancouver.ca)>

**Cc:** Callow, Matthew <[Matthew.Callow@vancouver.ca](mailto:Matthew.Callow@vancouver.ca)>; Mulhall, Ben <[Ben.Mulhall@vancouver.ca](mailto:Ben.Mulhall@vancouver.ca)>

**Subject:** RE: 2 Quick Questions re: Smithe

Hi Everyone,

Just got off a call with Dave from FMS and they have confirmed that 28 U-racks have been manufactured for the Smithe St project.

However there is no timeline yet for the installs, they are behind schedule and it might take a while before they get to Smithe St. They have been struggling with manpower and are trying to get more hires soon. I've asked Dave to give a call if they do manage to visit Smithe St but it won't be until later this year.

Regards,

Josh

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**From:** Pablo, Joshua

**Sent:** Thursday, September 8, 2022 2:26 PM



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**To:** Dong, Billy <[billy.dong@vancouver.ca](mailto:billy.dong@vancouver.ca)>; Thorseth, Leif <[leif.thorseth@vancouver.ca](mailto:leif.thorseth@vancouver.ca)>  
**Cc:** Callow, Matthew <[matthew.callow@vancouver.ca](mailto:matthew.callow@vancouver.ca)>; Mulhall, Ben <[Ben.Mulhall@vancouver.ca](mailto:Ben.Mulhall@vancouver.ca)>  
**Subject:** RE: 2 Quick Questions re: Smithe

Thanks for the update Billy, I'll let you and everyone know FMS' timeline as soon as I get an update from them.

Cheers,

**Josh Pablo, EIT** | (he, him)  
Streets Furniture Coordinator | Street Activities  
Engineering Services | City of Vancouver  
[joshua.pablo@vancouver.ca](mailto:joshua.pablo@vancouver.ca)  
C: 778-951-6326

---

**From:** Dong, Billy <[Billy.Dong@vancouver.ca](mailto:Billy.Dong@vancouver.ca)>  
**Sent:** Thursday, September 8, 2022 2:10 PM  
**To:** Thorseth, Leif <[Leif.Thorseth@vancouver.ca](mailto:Leif.Thorseth@vancouver.ca)>  
**Cc:** Callow, Matthew <[Matthew.Callow@vancouver.ca](mailto:Matthew.Callow@vancouver.ca)>; Pablo, Joshua <[Joshua.Pablo@vancouver.ca](mailto:Joshua.Pablo@vancouver.ca)>;  
Mulhall, Ben <[Ben.Mulhall@vancouver.ca](mailto:Ben.Mulhall@vancouver.ca)>  
**Subject:** RE: 2 Quick Questions re: Smithe

We aren't doing any massive expansions right now, so I would say if we gave them a week or so lead up time it should be reasonable.

Maybe let us know when FMS has their timeline ready and we can work off of that with P.Ops so we can try to coordinate similar time frames of removals/installs.

-Billy

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**From:** Thorseth, Leif <[Leif.Thorseth@vancouver.ca](mailto:Leif.Thorseth@vancouver.ca)>  
**Sent:** Thursday, September 8, 2022 2:06 PM  
**To:** Dong, Billy <[Billy.Dong@vancouver.ca](mailto:Billy.Dong@vancouver.ca)>  
**Cc:** Callow, Matthew <[Matthew.Callow@vancouver.ca](mailto:Matthew.Callow@vancouver.ca)>; Pablo, Joshua <[Joshua.Pablo@vancouver.ca](mailto:Joshua.Pablo@vancouver.ca)>;  
Mulhall, Ben <[Ben.Mulhall@vancouver.ca](mailto:Ben.Mulhall@vancouver.ca)>  
**Subject:** RE: 2 Quick Questions re: Smithe

Hi Billy,

Typically, how long does Parking Operations need to remove parking meters and poles? Josh will be providing a timeline from FMS to install bike racks on Smithe St and to my knowledge we would like the meters/poles removed the same day or shortly after as suggested below.

As Ben stated below, this is a bit confusing so maybe a quick meeting is needed to discuss.

Let me know if you have any questions.  
Thanks



**Leif Thorseth**

Project Coordinator

| Street Activities

Engineering Services | City of Vancouver

604-829-9425 | [leif.thorseth@vancouver.ca](mailto:leif.thorseth@vancouver.ca)

I am humbly thankful that I live and work on the territories of the xʷməθkʷəʔəm (Musqueam), Skwxwú7mesh (Squamish), and səliwətaʔ / səliwítlh (Tsleil-Waututh) nations.

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**From:** Mulhall, Ben <[Ben.Mulhall@vancouver.ca](mailto:Ben.Mulhall@vancouver.ca)>

**Sent:** Thursday, September 8, 2022 1:04 PM

**To:** Thorseth, Leif <[Leif.Thorseth@vancouver.ca](mailto:Leif.Thorseth@vancouver.ca)>; Pablo, Joshua <[Joshua.Pablo@vancouver.ca](mailto:Joshua.Pablo@vancouver.ca)>

**Cc:** Callow, Matthew <[Matthew.Callow@vancouver.ca](mailto:Matthew.Callow@vancouver.ca)>

**Subject:** FW: 2 Quick Questions re: Smithe

Hi Leif, Josh,

Sorry I should have forwarded this when I received it! Please see below regarding how to split the charges for the Smithe st. rack order and coordination of meter post removals.

@Josh, can you please follow up on the bike rack order for Smithe with FMS to confirm total quantities (see my email below from July 18<sup>th</sup>) and timeline for install?

@Leif, can you please follow up with Billy to coordinate meter post removals with Josh's work order to install racks?

This is a confusing one so please keep us all cc'd and feel free to set a meeting to clarify anything.

Regards,

**Ben Mulhall**

Landscape Architect | Street Activities

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**From:** Darwent, Christopher <[Christopher.Darwent@vancouver.ca](mailto:Christopher.Darwent@vancouver.ca)>

**Sent:** Monday, August 22, 2022 11:34 AM

**To:** Finlayson, Adam <[Adam.Finlayson@vancouver.ca](mailto:Adam.Finlayson@vancouver.ca)>; Gould, Brian <[Brian.Gould@vancouver.ca](mailto:Brian.Gould@vancouver.ca)>; Mulhall, Ben <[Ben.Mulhall@vancouver.ca](mailto:Ben.Mulhall@vancouver.ca)>

**Cc:** Duggan, Ann <[Ann.Duggan@vancouver.ca](mailto:Ann.Duggan@vancouver.ca)>; Dong, Billy <[Billy.Dong@vancouver.ca](mailto:Billy.Dong@vancouver.ca)>

**Subject:** RE: 2 Quick Questions re: Smithe

Hi Ben,

Sorry for the delay on this – I'll answer your two questions:

The charges for the 28 racks that were already manufactured should have gone to the Smithe St



project network # CER1372802.

From the email chain below, I understand any remaining charges, including installation labour, should go to the Post + Ring project network # CERD90857.

Can you please confirm this is acceptable?

Yes, this is acceptable for this instance as it is in line with the other "in-progress" work that we had going on when Cllr. Fry's motion was passed. In other words, it's additional scope that the meter project will absorb.

In the future, however, the project can't fund anymore beyond what was committed to earlier.

Can you tell me who should issue the work request to Parking Ops for the 9 meter removals?

Please have Leif work with Billy on this.

It will be important to do the removal around the same time the posts are removed.

Otherwise, it may optically look like we are removing bike parking if we wait too long with the removal.

I'm not sure how busy Parking Ops is at this point with on-boarding the new vendor, so that may be a bottleneck.

Hope this helps.

Chris

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**From:** Gould, Brian <[Brian.Gould@vancouver.ca](mailto:Brian.Gould@vancouver.ca)>

**Sent:** Friday, August 19, 2022 4:43 PM

**To:** Darwent, Christopher <[Christopher.Darwent@vancouver.ca](mailto:Christopher.Darwent@vancouver.ca)>

**Subject:** FW: 2 Quick Questions re: Smithe

Hi Chris,

I feel like this is actually a question better suited for you in your past life, if he's still available.

---

**From:** Finlayson, Adam <[Adam.Finlayson@vancouver.ca](mailto:Adam.Finlayson@vancouver.ca)>

**Sent:** August 16, 2022 8:22 AM

**To:** Gould, Brian <[Brian.Gould@vancouver.ca](mailto:Brian.Gould@vancouver.ca)>

**Subject:** 2 Quick Questions re: Smithe

Hi Brian,

I am sorry to be a bother; can you please provide some quick guidance on the two highlighted portions below?

If there is someone else that I should ask please let me know 😊.

Thank you!



Adam

---

**From:** Finlayson, Adam  
**Sent:** Friday, July 29, 2022 10:33 AM  
**To:** Gould, Brian <[brian.gould@vancouver.ca](mailto:brian.gould@vancouver.ca)>  
**Cc:** Mulhall, Ben <[Ben.Mulhall@vancouver.ca](mailto:Ben.Mulhall@vancouver.ca)>  
**Subject:** RE: Sleeve options for bike racks

Hi Brian,

Can you please confirm/provide quick guidance on the two highlighted portions below?

Thank you!

Adam Finlayson | M.Sc Planning, BLA, ISA Certified & TRAQ  
[adam.finlayson@vancouver.ca](mailto:adam.finlayson@vancouver.ca)  
604 871 6908

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**From:** Mulhall, Ben <[Ben.Mulhall@vancouver.ca](mailto:Ben.Mulhall@vancouver.ca)>  
**Sent:** Monday, July 18, 2022 2:37 PM  
**To:** Finlayson, Adam <[Adam.Finlayson@vancouver.ca](mailto:Adam.Finlayson@vancouver.ca)>; Darwent, Christopher <[Christopher.Darwent@vancouver.ca](mailto:Christopher.Darwent@vancouver.ca)>; Gould, Brian <[Brian.Gould@vancouver.ca](mailto:Brian.Gould@vancouver.ca)>; Reisig, Devin <[Devin.Reisig@vancouver.ca](mailto:Devin.Reisig@vancouver.ca)>  
**Cc:** Thorseth, Leif <[Leif.Thorseth@vancouver.ca](mailto:Leif.Thorseth@vancouver.ca)>; Callow, Matthew <[Matthew.Callow@vancouver.ca](mailto:Matthew.Callow@vancouver.ca)>; Pablo, Joshua <[Joshua.Pablo@vancouver.ca](mailto:Joshua.Pablo@vancouver.ca)>; Crowther, Anastasia <[Anastasia.Crowther@vancouver.ca](mailto:Anastasia.Crowther@vancouver.ca)>  
**Subject:** RE: Sleeve options for bike racks

Hi all,

Following up on this:

From what I understand, FMS has already manufactured 28 inverted U racks for the Smithe St project (Matthew ordered months ago). The attached revised plan includes replacement U racks for parking meters and conversion of one parking meter to post+ring.

My count for the work request is as follows:

- ☐ 33 standard inverted U racks
- ☐ 6 inverted U racks on bike corrals (2 corrals)
- ☐ 1 post + ring converted parking meter
- ☐ 9 parking meter removals



The charges for the 28 racks that were already manufactured should have gone to the Smithe St project network # CER1372802.

From the email chain below, I understand any remaining charges, including installation labour, should go to the Post + Ring project network # CERD90857.

Can you please confirm this is acceptable?

If so, we can issue the work request to FMS for the rack installs and manufacturing anything that wasn't already made.

Can you tell me who should issue the work request to Parking Ops for the 9 meter removals?

Thanks,

**Ben Mulhall**

Landscape Architect | Street Activities

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**From:** Finlayson, Adam <[Adam.Finlayson@vancouver.ca](mailto:Adam.Finlayson@vancouver.ca)>

**Sent:** Thursday, June 23, 2022 3:11 PM

**To:** Darwent, Christopher <[Christopher.Darwent@vancouver.ca](mailto:Christopher.Darwent@vancouver.ca)>; Gould, Brian <[Brian.Gould@vancouver.ca](mailto:Brian.Gould@vancouver.ca)>; Mulhall, Ben <[Ben.Mulhall@vancouver.ca](mailto:Ben.Mulhall@vancouver.ca)>; Callow, Matthew <[Matthew.Callow@vancouver.ca](mailto:Matthew.Callow@vancouver.ca)>; Reisig, Devin <[Devin.Reisig@vancouver.ca](mailto:Devin.Reisig@vancouver.ca)>

**Cc:** Thorseth, Leif <[Leif.Thorseth@vancouver.ca](mailto:Leif.Thorseth@vancouver.ca)>

**Subject:** RE: Sleeve options for bike racks

Hi All,

I just would like to check in on the bike racks for this project.

- Attached are the latest plans which include the parking meter to bike rack conversion on the last page

- In regards to Chris's question, I am not able to speak to the importance of the type of bike racks (i.e. U-rack, circle on converted meter, or other), maybe Brian or Ben could detail this?

Are there any outstanding items that I can provide to help move this along?

Thank you!  
Adam

Adam Finlayson | M.Sc Planning, BLA  
[adam.finlayson@vancouver.ca](mailto:adam.finlayson@vancouver.ca)



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**From:** Darwent, Christopher <[Christopher.Darwent@vancouver.ca](mailto:Christopher.Darwent@vancouver.ca)>  
**Sent:** Friday, June 3, 2022 1:50 PM  
**To:** Gould, Brian <[Brian.Gould@vancouver.ca](mailto:Brian.Gould@vancouver.ca)>; Finlayson, Adam <[Adam.Finlayson@vancouver.ca](mailto:Adam.Finlayson@vancouver.ca)>; Mulhall, Ben <[Ben.Mulhall@vancouver.ca](mailto:Ben.Mulhall@vancouver.ca)>; Callow, Matthew <[Matthew.Callow@vancouver.ca](mailto:Matthew.Callow@vancouver.ca)>; Reisig, Devin <[Devin.Reisig@vancouver.ca](mailto:Devin.Reisig@vancouver.ca)>  
**Cc:** Thorseth, Leif <[Leif.Thorseth@vancouver.ca](mailto:Leif.Thorseth@vancouver.ca)>  
**Subject:** RE: Sleeve options for bike racks

How important is it that we use U-racks? The reason I ask is that FMS came up with a way to reuse the meter posts that we have to remove in combination with some rings that we have already produced. This is a less expensive option.

Regardless, this isn't a huge expense either way, so Leif/Ben/Mathew we can include this with the other additional work we are doing as a result of Fry's request.

Thanks,

Chris

---

**From:** Gould, Brian <[Brian.Gould@vancouver.ca](mailto:Brian.Gould@vancouver.ca)>  
**Sent:** Friday, June 3, 2022 9:26 AM  
**To:** Finlayson, Adam <[Adam.Finlayson@vancouver.ca](mailto:Adam.Finlayson@vancouver.ca)>; Mulhall, Ben <[Ben.Mulhall@vancouver.ca](mailto:Ben.Mulhall@vancouver.ca)>; Callow, Matthew <[Matthew.Callow@vancouver.ca](mailto:Matthew.Callow@vancouver.ca)>; Darwent, Christopher <[Christopher.Darwent@vancouver.ca](mailto:Christopher.Darwent@vancouver.ca)>; Reisig, Devin <[Devin.Reisig@vancouver.ca](mailto:Devin.Reisig@vancouver.ca)>  
**Cc:** Thorseth, Leif <[Leif.Thorseth@vancouver.ca](mailto:Leif.Thorseth@vancouver.ca)>  
**Subject:** RE: Sleeve options for bike racks

Hi all,

I would like to take Chris up on his offer to fund the block affected by meter replacements (it's actually 1000 and not 1100, right?). In our TransLink funding application we did indicate bike racks would be a feature, and to me that means some geographic spread and some net increase versus the meter increase, ergo we should make something happen on the remaining blocks – if that's a significant number and the ask is to TDE, please let me know and let's get it in front of Alex. But to my eye this is an equivalent deployment to what we put in through the 2016 quick-builds, maybe even a bit less since Smithe is bidirectional.

---

**From:** Finlayson, Adam <[Adam.Finlayson@vancouver.ca](mailto:Adam.Finlayson@vancouver.ca)>  
**Sent:** May 31, 2022 4:10 PM  
**To:** Mulhall, Ben <[Ben.Mulhall@vancouver.ca](mailto:Ben.Mulhall@vancouver.ca)>; Callow, Matthew <[Matthew.Callow@vancouver.ca](mailto:Matthew.Callow@vancouver.ca)>; Darwent, Christopher <[Christopher.Darwent@vancouver.ca](mailto:Christopher.Darwent@vancouver.ca)>; Gould, Brian <[Brian.Gould@vancouver.ca](mailto:Brian.Gould@vancouver.ca)>; Reisig, Devin <[Devin.Reisig@vancouver.ca](mailto:Devin.Reisig@vancouver.ca)>  
**Cc:** Thorseth, Leif <[Leif.Thorseth@vancouver.ca](mailto:Leif.Thorseth@vancouver.ca)>



---

**Subject: RE: Sleeve options for bike racks**

Hi Ben,

Please see the attached for an updated plan that includes parking meter conversions and their 1-1 replacements, particularly the last page of the PDF. Thanks to Matthew and the productive conversation we had earlier today, we confirmed which parking meters can and can't be converted, and also found room for an additional 5 bike racks on the block between Thurlow and Burrard. This honors the 1-1 replacement and brings the new total to 33 U-Bike Racks with one parking meter pole being converted to a 'circle' bike rack.

1100 Block Smithe Parking Meter Conversion Count

- o Total Parking Meters: 10
- o Parking Meters for Conversion: 1
- o Parking Meters for one-to-one replacement with U Bike Rack: 9
- o Bike Racks proposed on this block: 9 (see detail 1 & 2 on LP06; and Detail 12 & 13 on LP11)

I unfortunately cannot speak to the funding of this and will ask Brian to weigh in.

Thank you!

Adam Finlayson | M.Sc Planning, BLA  
[adam.finlayson@vancouver.ca](mailto:adam.finlayson@vancouver.ca)  
604 871 6908

---

**From:** Mulhall, Ben <[Ben.Mulhall@vancouver.ca](mailto:Ben.Mulhall@vancouver.ca)>  
**Sent:** Monday, May 30, 2022 1:03 PM  
**To:** Callow, Matthew <[Matthew.Callow@vancouver.ca](mailto:Matthew.Callow@vancouver.ca)>; Finlayson, Adam <[Adam.Finlayson@vancouver.ca](mailto:Adam.Finlayson@vancouver.ca)>; Darwent, Christopher <[Christopher.Darwent@vancouver.ca](mailto:Christopher.Darwent@vancouver.ca)>; Gould, Brian <[Brian.Gould@vancouver.ca](mailto:Brian.Gould@vancouver.ca)>; Reisig, Devin <[Devin.Reisig@vancouver.ca](mailto:Devin.Reisig@vancouver.ca)>  
**Cc:** Thorseth, Leif <[Leif.Thorseth@vancouver.ca](mailto:Leif.Thorseth@vancouver.ca)>  
**Subject:** RE: Sleeve options for bike racks

Hi Adam,

We have asked FMS to start manufacturing 28 new racks for the Smith St bike lane as indicated on the design drawings.

Please confirm what the new total is, including the replacements for parking meters. A single revised drawing with all locations would be helpful for the installation work order for FMS (assuming it is okay to charge them all to the same budget).

@Chris, note there will be significantly more racks on this stretch from the bike lane project, so the



spirit of the Council direction may be met here even without parking meter conversion. However, I'd still support converting the parking meters since the opportunity is there!

Regards,

**Ben Mulhall**

Landscape Architect | Street Activities

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**From:** Callow, Matthew <[Matthew.Callow@vancouver.ca](mailto:Matthew.Callow@vancouver.ca)>

**Sent:** Monday, May 30, 2022 12:18 PM

**To:** Finlayson, Adam <[Adam.Finlayson@vancouver.ca](mailto:Adam.Finlayson@vancouver.ca)>; Darwent, Christopher <[Christopher.Darwent@vancouver.ca](mailto:Christopher.Darwent@vancouver.ca)>; Gould, Brian <[Brian.Gould@vancouver.ca](mailto:Brian.Gould@vancouver.ca)>; Reisig, Devin <[Devin.Reisig@vancouver.ca](mailto:Devin.Reisig@vancouver.ca)>

**Cc:** Thorseth, Leif <[Leif.Thorseth@vancouver.ca](mailto:Leif.Thorseth@vancouver.ca)>; Mulhall, Ben <[Ben.Mulhall@vancouver.ca](mailto:Ben.Mulhall@vancouver.ca)>

**Subject:** RE: Sleeve options for bike racks

Hi all,

I think Adam has done a good job interpreting the bike parking guidelines, and I agree that most of the existing locations are not feasible given the requirement to maintain 1.8m of clear sidewalk width.

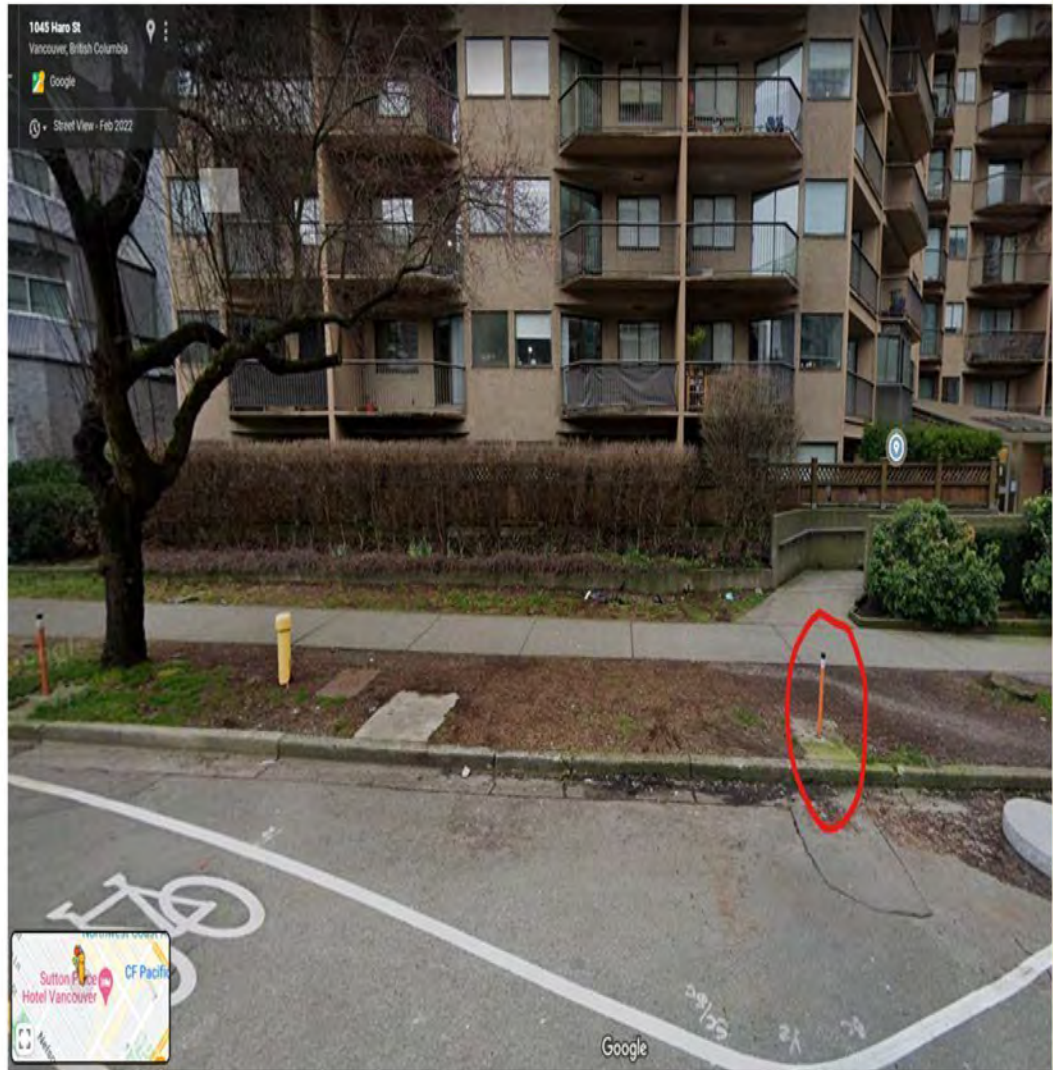
A few comments and options I see to ensure all 9 parking meters on this block are replaced with bike racks:

1.

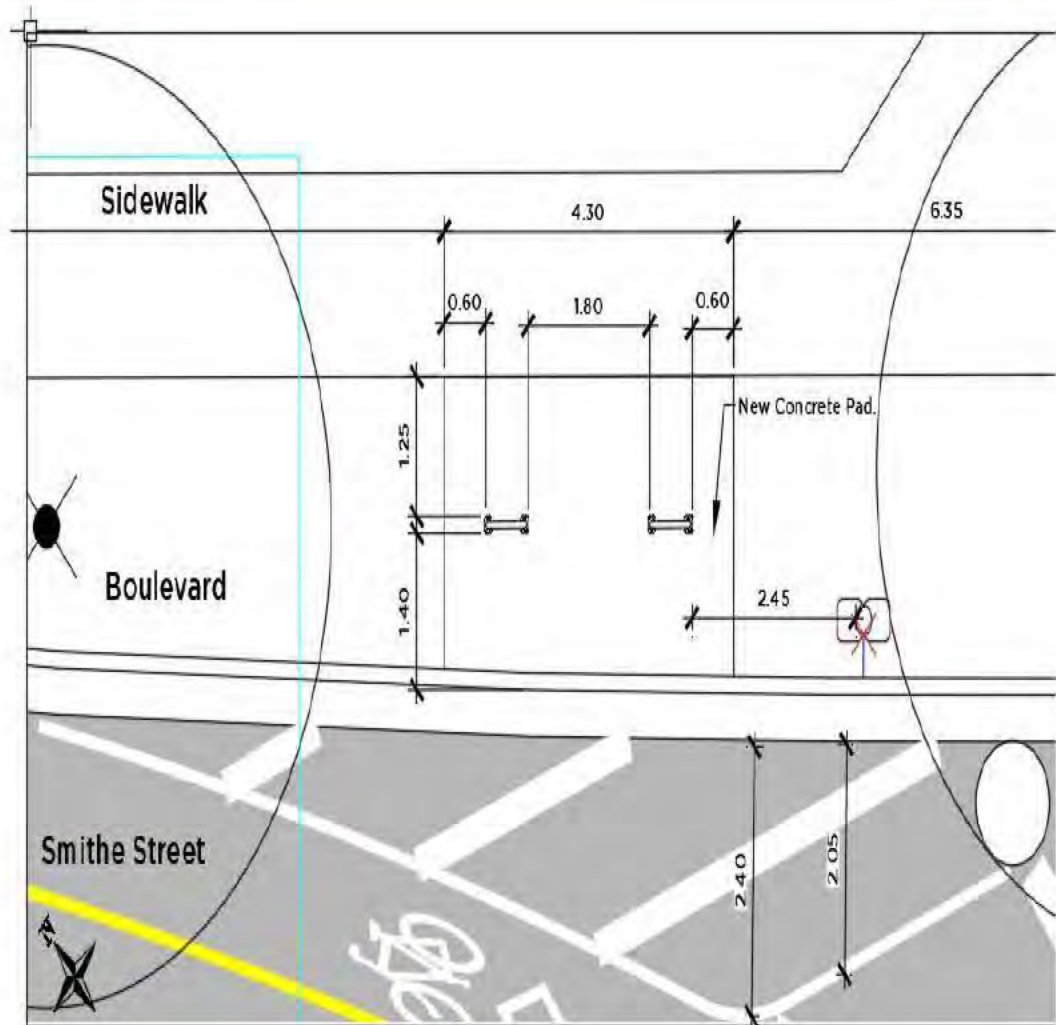
I agree there is space in the utility strip where Haro intersects with Smithe and bike rack layout 1 in the landscape plan looks good. Can anyone confirm if the parking meter pole is still in place and if so, what is the distance from the parking meter pole to the front of the curb? With the 2 installs as part of the Smithe Bikeway this location likely isn't needed anyway.

Based on the street view image below the parking meter pole was in place as of February 2022. The distance between the parking meter pole and the front of the curb is ~.45 meters, when considering this distance it may be important to note that there is ~ 2 meters of buffer space between the curb and bike lane/driveway exit. Another relevant dimension is the ~2.5 meters between the proposed adjacent U-Rack and the parking meter pole.









1 **Bike Rack Layout**  
LP-06 Scale: 1:50

2. The parking meter in front of Le Crocodile looks a bit close to the tree as is. Adam - do you know the distance from the pole to the tree well? If this location is too close to the tree we could shift it slightly and that would work.

The distance between parking meter pole and the tree well is 1.1 meters.

3. There is space on Smithe closer to Burrard for potentially 4 bike racks as there is a utility strip. (Space in front of the IGA for 2 bike racks and possibly space outside Sutton Place Wine Merchant for 2 bike racks).



4.  
Good to know there are also 2 bike racks going in near the Mobi station near Burrard.
5.  
Those suggestions would get us to 9 bike racks on this block. Another option is to install a 3-rack bicycle corral in place of a car parking space. Chris is likely better to advise if this is a possibility on this block.

Hopefully this is helpful! Please let me know if you have any additional questions. I am also happy to set up a quick meeting to discuss as well if that helps!

PS. Brian – I am going through this same review process for many other blocks and hope to have a number of parking meters converted 1:1 along 200 E Pender soon!

Thanks,  
Matthew

**Matthew Callow** (he/him) MCRP  
Planning Analyst | Street Activities  
Engineering Services | City of Vancouver  
604-871-6339 | [matthew.callow@vancouver.ca](mailto:matthew.callow@vancouver.ca)

Acknowledging the unceded territories of the xʷməθkʷəy̓əm (Musqueam), Skwxwú7mesh (Squamish), and səliwətał (Tsleil-Waututh) Nations

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**From:** Finlayson, Adam <[Adam.Finlayson@vancouver.ca](mailto:Adam.Finlayson@vancouver.ca)>  
**Sent:** Thursday, May 26, 2022 4:56 AM  
**To:** Darwent, Christopher <[Christopher.Darwent@vancouver.ca](mailto:Christopher.Darwent@vancouver.ca)>; Gould, Brian <[Brian.Gould@vancouver.ca](mailto:Brian.Gould@vancouver.ca)>; Reisig, Devin <[Devin.Reisig@vancouver.ca](mailto:Devin.Reisig@vancouver.ca)>  
**Cc:** Callow, Matthew <[Matthew.Callow@vancouver.ca](mailto:Matthew.Callow@vancouver.ca)>; Thorseth, Leif <[Leif.Thorseth@vancouver.ca](mailto:Leif.Thorseth@vancouver.ca)>  
**Subject:** RE: Sleeve options for bike racks

Thanks Chris! I should have mentioned this in my previous email and can include now for context, the meters that I thought may create too narrow of a sidewalk if converted did not seem able to provide 1.8m clearance once a bike was locked to them (based on the attached); there is a good chance that I am not being flexible enough with that 1.8 m determination and I am glad that Leif and Matthew can review for that reason.

I have updated the previously sent VanDocs Document with the following info:

- - 1100 Block Smithe Parking Meter Conversion Count
    - For Conversion: 2
    -



For 1-1 Replacement: 7 (4 due to sidewalk narrowing <1.8 m; 3 due to existing obstructions)

○

Bonus Stat: There are 4 U-Racks being installed on this block as part of the Bikeway Upgrades (ETA 2022).

Thanks!  
Adam

---

**From:** Darwent, Christopher <[Christopher.Darwent@vancouver.ca](mailto:Christopher.Darwent@vancouver.ca)>  
**Sent:** Wednesday, May 25, 2022 6:00 PM  
**To:** Finlayson, Adam <[Adam.Finlayson@vancouver.ca](mailto:Adam.Finlayson@vancouver.ca)>; Gould, Brian <[Brian.Gould@vancouver.ca](mailto:Brian.Gould@vancouver.ca)>; Reisig, Devin <[Devin.Reisig@vancouver.ca](mailto:Devin.Reisig@vancouver.ca)>  
**Cc:** Callow, Matthew <[Matthew.Callow@vancouver.ca](mailto:Matthew.Callow@vancouver.ca)>; Thorseth, Leif <[Leif.Thorseth@vancouver.ca](mailto:Leif.Thorseth@vancouver.ca)>  
**Subject:** RE: Sleeve options for bike racks

Thanks Adam.

The best people to review these would be Mathew and Leif from Street Activities (they know our bike parking standards well).

In the meantime, could you give me a breakdown of the the number of meter posts that:

- a)  
Can be converted into bike racks, and
- b)  
Can't be converted into bike racks (we can look for other locations nearby to place a rack – the goal is to no lose any bike parking infrastructure, even the ad hoc stuff)

Chris

---

**From:** Finlayson, Adam <[Adam.Finlayson@vancouver.ca](mailto:Adam.Finlayson@vancouver.ca)>  
**Sent:** Wednesday, May 25, 2022 4:42 PM  
**To:** Darwent, Christopher <[Christopher.Darwent@vancouver.ca](mailto:Christopher.Darwent@vancouver.ca)>; Gould, Brian <[Brian.Gould@vancouver.ca](mailto:Brian.Gould@vancouver.ca)>; Reisig, Devin <[Devin.Reisig@vancouver.ca](mailto:Devin.Reisig@vancouver.ca)>  
**Subject:** RE: Sleeve options for bike racks

*This time with both attachments that were referenced – thanks!*

Adam Finlayson | M.Sc Planning, BLA  
[adam.finlayson@vancouver.ca](mailto:adam.finlayson@vancouver.ca)  
604 871 6908

---

**From:** Finlayson, Adam  
**Sent:** Wednesday, May 25, 2022 1:00 PM  
**To:** Darwent, Christopher <[Christopher.Darwent@vancouver.ca](mailto:Christopher.Darwent@vancouver.ca)>; Gould, Brian <[brian.gould@vancouver.ca](mailto:brian.gould@vancouver.ca)>; Reisig, Devin <[devin.reisig@vancouver.ca](mailto:devin.reisig@vancouver.ca)>  
**Subject:** RE: Sleeve options for bike racks

Hi All,

Attached is a quick analysis of what parking meters on the 1000 block of Smithe could be converted to bike racks. Please let me know if this is helpful, or how it could be adjusted. The conversion locations do not interrupt the Smithe Street Bike Parking Plan (also attached) and therefore we are planning on having this implemented as per sending it to Street Activities for implementation.

Thank you!

Adam Finlayson | M.Sc Planning, BLA  
[adam.finlayson@vancouver.ca](mailto:adam.finlayson@vancouver.ca)  
604.871.6908

---

**From:** Darwent, Christopher <[Christopher.Darwent@vancouver.ca](mailto:Christopher.Darwent@vancouver.ca)>  
**Sent:** Monday, May 2, 2022 9:26 AM  
**To:** Gould, Brian <[Brian.Gould@vancouver.ca](mailto:Brian.Gould@vancouver.ca)>; Reisig, Devin <[Devin.Reisig@vancouver.ca](mailto:Devin.Reisig@vancouver.ca)>  
**Cc:** Finlayson, Adam <[Adam.Finlayson@vancouver.ca](mailto:Adam.Finlayson@vancouver.ca)>  
**Subject:** RE: Sleeve options for bike racks

So , we were aiming to replace about 30-40% of meter posts with retrofit bike racks. In our opinion, this is about all we could do due to narrow sidewalks and other obstructions.

That said, Council passed a motion two weeks ago for us to do more as well as come up with a bike parking plan.

For parking meter replacements going forward, we are going to be bagging the disused parking meters until the final bike parking plan is finished. This way no ad hoc bike parking infrastructure, even if it isn't quite in the right place, will be lost.

We do, however, have a handful of these half-done locations. Given the sentiment of Council, we want to make sure that there is no loss in any bike parking opportunities, even unlawful ones, at this time. Thus my statement below that we will replace every parking meter with a bike rack one-to-one.

If you are worried about the funding, the parking meter project can pay for it. We are also working with Street Activities on this – they are the ones that are determining if a parking meter is suitable for retrofit, or not.

We could just include this in their work.

---



Chris

---

**From:** Gould, Brian <[Brian.Gould@vancouver.ca](mailto:Brian.Gould@vancouver.ca)>  
**Sent:** Monday, May 2, 2022 9:10 AM  
**To:** Darwent, Christopher <[Christopher.Darwent@vancouver.ca](mailto:Christopher.Darwent@vancouver.ca)>; Reisig, Devin <[Devin.Reisig@vancouver.ca](mailto:Devin.Reisig@vancouver.ca)>  
**Cc:** Finlayson, Adam <[Adam.Finlayson@vancouver.ca](mailto:Adam.Finlayson@vancouver.ca)>  
**Subject:** RE: Sleeve options for bike racks

Hi folks,

Adam is considering both these locations when it comes to bike parking and I'd like to understand whether we might be better off with U racks serving similar locations. Probably not all of the south side of Beach is equally valuable, and I think 1000 Smithe for example has viable U rack locations that reduce sidewalk impacts, but these are all appropriate blocks to be considering.

Wondering what the process is when we're not on an obvious bike route (e.g. was disappointed last weekend when visiting 200 E Pender to no longer even have a meter to park to)

---

**From:** Darwent, Christopher <[Christopher.Darwent@vancouver.ca](mailto:Christopher.Darwent@vancouver.ca)>  
**Sent:** May 2, 2022 9:04 AM  
**To:** Reisig, Devin <[Devin.Reisig@vancouver.ca](mailto:Devin.Reisig@vancouver.ca)>; Gould, Brian <[Brian.Gould@vancouver.ca](mailto:Brian.Gould@vancouver.ca)>  
**Subject:** RE: Sleeve options for bike racks

Unless Brian Gould, EOR on these projects, wants them cut off, we are going to want to turn these into bike racks.

For locations that aren't suitable for a retrofit, we will replace nearby one-to-one.

Brian, can your project pay for this? It's about 7k.

---

**From:** Reisig, Devin <[Devin.Reisig@vancouver.ca](mailto:Devin.Reisig@vancouver.ca)>  
**Sent:** Sunday, May 1, 2022 9:30 AM  
**To:** Darwent, Christopher <[Christopher.Darwent@vancouver.ca](mailto:Christopher.Darwent@vancouver.ca)>  
**Subject:** Sleeve options for bike racks

Hey Chris,

We have 20+ sleeves to cut off from the Beach Ave bike way project and the S/S900, 1100, 1200, 1300 Beach Ave (approx. 25)

- N/S 1800 Beach Ave (3)
- N/S 1800 Morton (3)



---

We also have a few on N/S 1000 Smithe

Do we want these turned into bike racks? Or should we cut them off?

byeeee

**Devin Reisig**

*(he/him)*

**Superintendent I**

|

**Parking Operations**

**City of Vancouver**

[Devin.Reisig@Vancouver.ca](mailto:Devin.Reisig@Vancouver.ca)

Desk: 604-871-6041

Cell: 604-603-2362



**From:** "Reisig, Devin" <Devin.Reisig@vancouver.ca>  
**To:** "DeSanti, Mark" <Mark.DeSanti@vancouver.ca>  
**Date:** 3/9/2023 2:36:23 PM  
**Subject:** FW: Ryan Petrin streets 1000 Smithe st

---

**From:** Sirsiris, George <george.sirsiris@vancouver.ca>  
**Sent:** Friday, September 24, 2021 1:55 PM  
**To:** Rai, Ran <Ran.Rai@vancouver.ca>; POps - Supervisor - DL s.15(1)(l)  
**Cc:** Manitoba Yard Central Dispatchers s.15(1)(l)  
**Subject:** RE: Ryan Petrin streets 1000 Smithe st

Received, thank you.

George Sirsiris  
Parking Ops. Supervisor  
t: 604.871.6226



---

**From:** Rai, Ran <Ran.Rai@vancouver.ca>  
**Sent:** Friday, September 24, 2021 1:46 PM  
**To:** POps - Supervisor - DL s.15(1)(l)  
**Cc:** Manitoba Yard Central Dispatchers s.15(1)(l)  
**Subject:** Ryan Petrin streets 1000 Smithe st

Hi

I have a request to remove meters , please contact Forman to get all fine details

Ryan Petrin  
Streets  
1000 Smithe St  
604-690-5595  
CERD92807

Remove Meters  
21-10-43/49  
13-10-19/05/03  
13-07-03/05

Also asking if the meter tech's can core the sleeves out.



Will be putting islands in around 3/4 Oct

Ran

Contract Services Coordinator

s.15(1)(l)

604-326-4820



# PARKING METER REMOVALS + REINSTALLS

DATE

LOCATION

# OF HOUSINGS  
REMOVED  
TW + SING

JAN 20, 2022

1000 SMITHE ST NS

2TW + 1SING

REMOVED OR  
BY

REASON FOR REMOVAL

CHARGE #

REFERENCE  
MARKS

\* 4m

PERMANENT REMOVAL FOR  
BIKE LANE

\*  
CERD92807

Y OR N

CITY CREW

METER CONDITION

SLEEVES

RECEMENTS

TRUCK B1620

DAMAGED

DIRTY

TO

WORK REQUIRED

CONTACT

CONCRETE  
ATTACHED

ACCEPTABLE

# OF

CHIPPER

DEPT PARKING OPS

COMMENTS

DATE

JAN 20, 2022

CORING

WO#

PAD WORK

SUGGESTED REINSTALL DATE

SITE INSPECTION

DATE AND COMMENTS

REVISED REINSTALL DATE

SITE INSPECTION

DATE AND COMMENTS

HEADS REFURBISHED

\*

METER AND MECHANISM ATTRIBUTES

PIPE INSTALLED

DATE

METER #

PBP #

LOCK

MECHS

PROHIBITIONS

DATE

13-10-01/03 63676/64065 #11 2P4D2HX  
13-10-05 63587 #11 "  
13-10-19/21 93566/64204 #11 "

ORANGE PVC  
JAN 20, 2022

INITIALS

INITIALS

4m

HEADS BUILT

METERS INSTALLED

DATE

DATE

INITIALS

INITIALS

## OFFICE USE ONLY

ATTRIBUTES

INVENTORY

TICKET MGR

TOPOBASE

METERBOARD

TOTAL SPACE

SUBZONE

PAY BY PHONE



# PARKING METER REMOVALS + REINSTALLS

DATE

LOCATION

# OF HOUSINGS  
REMOVED  
TW + SING

JAN 20, 2022

1000 SMITH ST NS

2TW + 1SING

REMOVED OR  
BY  
\* 4m

REASON FOR REMOVAL

PERMANENT REMOVAL FOR  
BIKE LANE

CHARGE #

\* CERD92807

REFERENCE  
MARKS

Y OR N

CITY CREW

METER CONDITION

SLEEVES

RECEMENTS

TRUCK B1620

DAMAGED

DIRTY

CONCRETE  
ATTACHED

ACCEPTABLE

COMMENTS

CONTACT

TO

# OF

3

DATE

JAN 20, 2022

WORK REQUIRED

CHIPPER

CORING

PAD WORK

DEPT PARKING OPS

WO#

SUGGESTED REINSTALL DATE

SITE INSPECTION

DATE AND COMMENTS

REVISED REINSTALL DATE

SITE INSPECTION

DATE AND COMMENTS

HEADS REFURBISHED

\*

METER AND MECHANISM ATTRIBUTES

PIPE INSTALLED

DATE

METER #

PBP #

LOCK

MECHS

PROHIBITIONS

DATE

10' PIPE →  
IN NOW  
TAKE OUT  
AND CALL  
GEORGE.

13-10-01/03 63676/64065 #11 2P4D2HX  
13-10-05 63587 #11 "  
13-10-19/21 93566/64204 #11 "

ORANGE PVC  
JAN 20, 2022

INITIALS

4m

GEORGE.

(65) MAR 8, 2023

CUT + FILL  
ALL (3) SLEEVES  
ABOVE  
4m/BH  
COMPLETED MAR 8, 2023

HEADS BUILT

METERS INSTALLED

DATE

DATE

INITIALS

INITIALS

OFFICE USE ONLY

ATTRIBUTES

INVENTORY

TICKET MGR

TOPOBASE

METERBOARD

TOTAL SPACE

SUBZONE

PAY BY PHONE





# SIDEWALK HAZARD INSPECTION PROGRAM

## Inspection Manual and Procedures

Engineering Services  
City of Vancouver

Revision: 1.4  
Date: February 8, 2023



## Revision History

Rev.	Date	Comments
0	2012-11-28	Draft issued for review
1.0	2013-12-24	Issued for 2013-2014 Sidewalk Inspection Program
1.1	2014-12-19	Issued for 2014-2015 Sidewalk Inspection Program; revised associated sidewalks diagram, revised Landscape Encroachment program procedures
1.2	2016-01-15	Issued for 2015-2016 Sidewalk Inspection Program; revised Sidewalk Obstacle description
1.3	2019-01-9	Issued for 2019 Sidewalk Inspection Program; Addition of Sidewalk Condition Rating
1.4	2023-02-08	Issued for 2023 Sidewalk Inspection Program; Addition of Sidewalk Condition Rating



## DEFINITIONS

**Annual Inspection:** A scheduled inspection program that is carried out approximately once a year. Some variance may occur due to availability of resources and other factors.

**Boulevard:** The area between a roadway's shoulder or curb & gutter and the adjacent property line that is not a sidewalk or pathway, and on a street where traffic is separated by means of a median, and includes the median. This area may be landscaped with grass, trees, plantings, other installed features and/or private encroachments (e.g. connector walks, wooden curbs, landscaping, etc.) and there may be an inherent variability of the surface.

**Crosswalk:** The area where a pedestrian has the right of way when crossing a roadway that is either a marked pedestrian crossing or an area within an intersection that is the portion of a roadway between the extension of the lateral edge of the roadway and the adjacent lateral property line but does not include lane intersections.

**Curbs:** A permanent curb or curb & gutter, usually constructed from concrete, asphalt, or stone, that is installed at the edge of a roadway to separate a roadway pavement from a boulevard and/or sidewalk.

**Day:** a 24 hour period.

**Hazard:** A defect that exceeds the tolerance specified for an asset.

**Inspector:** A person who has been assigned by the City to undertake the inspection of an asset in accordance with this inspection program.

**Lane:** A street less than 10.06 metres in width that is usually located at the rear of a property.

**Pathway:** A pedestrian or shared pedestrian path (a path that is intended for use by pedestrians and other non-motorized traffic), which is neither a roadway nor a sidewalk, which has been improved by the City with a permanent hard surface (such as concrete, asphalt, or pavers).

**Paved Surface:** A surface constructed with a layer or layers of asphalt, concrete, or pavers.

**Roadway:** The portion of a street improved, designed, or intended for vehicular use and located between curbs and/or shoulders.

**Sidewalk:** The portion of a street, improved for the use of pedestrians, between the curb lines or lateral lines of a roadway and the adjacent property lines. A sidewalk is improved with a permanent hard surface (such as concrete, asphalt, or pavers) that is intended for the primary use of pedestrians, including the main sidewalk surface, accessibility curb ramps, bus stop landings, and portions of a sidewalk that cross a lane entrance



**Shoulder:** The portion of a street between the roadway paved surface and the boulevard, usually without a permanent paved surface and where a curb has not been installed, that provides lateral support to the roadway and may accommodate stopped vehicles.

**Street:** A public road, highway, bridge, viaduct, lane and sidewalk, and any other way normally open to the use of the public, but does not include a private right-of-way on private property.

**Trail:** A granular or bark mulch sidewalk or pathway installed by the City that is intended for use by pedestrians and/or non-motorized traffic. A trail does not have a permanent paved surface and therefore there is an inherent variability of the surface.

**Tree Base:** The area around the trunk of a tree that is located within a sidewalk. The area may have a natural surface or may have a manufactured cover installed surrounding the tree.

**Tree Grate:** A manufactured cover, typically constructed from metal or concrete, that is installed around a tree to protect the tree base. Also may be referred to as a tree surround or a concrete surround.



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## 1.0 INTRODUCTION

The City of Vancouver has over 4200 kilometers of streets, lanes, sidewalks, and pathways throughout the City. These assets vary in age, construction and condition. Over time, defects may develop that could pose a hazard to pedestrians or road users.

The City has established a Street and Sidewalk Hazard Inspection Policy that specifies the requirements for assets to be inspected, the definitions of defects that are considered to be hazardous, and the requirements for responding to identified hazards so that a reasonable level of service is provided to the public.

The Sidewalk Hazard Inspection Program has been developed by Engineering Services to implement an “annual inspection” program in accordance with the Street and Sidewalk Hazard Inspection Policy for assets that are the maintenance responsibility of Engineering Services.

Inspections are carried out by inspectors assigned to the inspection program by Engineering Services. Inspections are scheduled based on the availability of resources for inspection services and resources of maintenance staff to undertake identified repairs.

## 2.0 SCOPE

The following assets are inspected as part of this scheduled “annual inspection” program. The exact timing of the inspection for an individual asset may vary from year to year within the inspection program:

- Sidewalks and Pathways
- Crosswalks
- Curbs directly abutting Sidewalks and Crosswalks

Only assets in the above categories that are the maintenance responsibility of Engineering Services are inspected as part of this program. City facilities, Parks, and right of ways on private property maintained by others are not inspected.

Steps and walkways at the following locations are inspected by others and are not inspected as part of this program:

- South end Burrard Bridge, one set each side
- North end Bayswater St to the beach
- North end Balaclava St to the beach
- North end Waterloo St to the beach
- Dunbar St/Cameron St walkway to the beach
- Northeast corner Trimble St/W 4th Av to Locarno
- West end W 3rd Av to Trimble St
- West end W 3rd Av to lane west of Trimble and lane south of W 4th Av
- South end Sasamat St to W 3rd Ave.
- 300 metres west of Blanca St, Belmont Av to NW Marine Dr
- Lane south of W 11th Av, Crown St to west
- Lane south of W 11th Av, Crown St to east



- Wallace St, lane south of W 10th Av, to W 10th Av
- West side Dunbar St to W 13th Av
- Northeast corner Dunbar St & W 14th Av to northeast
- West side of Blenheim St to 19th Av
- North of W King Edward Av, Puget Dr to lane west of Puget Dr
- Southwest corner W King Edward Av and Puget Dr to Quesnel Dr
- South side W 29th Av and Puget Dr
- South side Puget Dr to lane south of Puget Dr at Trafalgar St
- Trafalgar St, Puget Dr to lane south McBain Av
- W 29th Av, Maple Cr to lane east of Magnolia St
- Science Centre at the west side of Quebec St & Terminal Av
- False Creek (north side and south side)
- Coal Harbour (south side of Burrard Inlet)
- Laurel Street pedestrian bridge

### 3.0 INSPECTION PROCEDURES

Inspections are undertaken based on visual checks of the surface exposed at the time of inspection to identify hazards. When a hazard is identified in accordance with the policy, the inspection result will be recorded by the inspector and forwarded to maintenance staff so that a repair will be scheduled. The hazard shall be repaired within the timeframe specified for that asset. An asset is considered to be repaired when a repair has been undertaken so that the defect no longer meets the condition that defines the defect as a hazard.

In most cases, it will be appropriate for inspectors to walk each sidewalk once to identify defects on the inspected sidewalk and any inspected curbs directly abutting that sidewalk and/or associated crosswalks. On wide and heavily walked sidewalks (such as sidewalks in commercial areas or sidewalks adjacent to institutional properties) more than one pass may be appropriate. If leaves, snow, or other debris covers an asset that is to be inspected, such that an inspection of the surface condition is not reasonable, the inspection should be postponed until the surface can be observed.

Although the inspection of the surface exposed at the time of inspection is visual, inspectors will take a measurement of the identified hazards when there may be doubt to whether a hazard exceeds the tolerance specified for that defect. Inspectors will always carry a ruler for measurement during inspection.

In any circumstance when traffic may put the inspector in danger, the inspector must not walk into a roadway and/or traffic and must maintain their safety.

Inspections shall be recorded by the inspector and entered into the department's work management system, Hansen. The Hansen system will maintain the inspection records and will be used to track repairs associated with the inspection. Typically, inspectors will enter inspection records directly into the Hansen system through portable computing devices.



Inspection forms will be created for sidewalk and pathway assets in the Hansen system. Some of these assets will not be subject to inspection under the Street and Sidewalk Hazard Inspection Policy.

If an asset is subject to inspection, the inspector will undertake the required inspections on the asset and any associated assets, complete the inspection form, and close the inspection form once the inspection is completed.

If an asset is not subject to inspection but there are associated assets that are subject to inspection (such as crosswalks), the inspector will undertake the required inspections on the associated assets, complete the inspection form, and close the inspection form once the inspection is completed.

If an asset is not subject to inspection and there are no associated assets that are subject to inspection, the inspector will not undertake an inspection and will complete and close the inspection form.

When a sidewalk or an associated street asset (curbs and/or crosswalks) is inspected, an inspection record will be completed for the sidewalk asset. Defects found on the sidewalk will be recorded on the sidewalk inspection form and associated to the sidewalk asset. Defects found on an inspected street asset (curbs and/or crosswalks) will be recorded on the sidewalk inspection form and associated to the street asset.

When a pathway or associated pathway asset is inspected, defects found on the pathway or associated pathway asset will be recorded on the pathway inspection form and associated with the pathway asset.

The inspector will identify the type and location of an identified inspection defect on the inspection form. The inspection form allows for the recording of multiple defects on an asset. Identified inspection defects must be recorded on the form so that maintenance staff will be able to identify and locate defects that must be repaired. If there is more than one defect found at a specific location, the most predominant defect should be identified and recorded on the inspection form. When multiple defects are identified within a close proximity of each other, a single inspection defect may be entered into the inspection form to record the defects and to indicate the nature and location of the defects.

In the event that the Hansen system is not available, inspectors will manually record inspection results and enter the inspection records into the Hansen system as soon as practicable.

Typically the inspector will be able to report the defect location by using an approximate distance in meters from an adjacent reference location or landmark, such as a civic address, fire hydrant, pole, or lane entrance. In some limited cases it may be necessary for the inspector to provide further details to maintenance staff on the location of a defect, particularly in locations where it is difficult to describe the location of a defect in the inspection form. In these cases the inspector should contact the maintenance staff to ensure that the location of the defect is located for repair.



In the event that there is a discrepancy in the asset information in Hansen and the observed asset properties for an asset subject to inspection under this policy, the inspector will make note of the discrepancy in the data on the inspection record, will inform the asset owner of the discrepancy so that the asset information in Hansen can be updated, and will inspect the asset based on the observed asset properties.

During the undertaking of this sidewalk hazard inspection program the inspector may notice other defects or situations which they may feel may put the public in danger or that may need immediate attention. It is the expectation of the City that when staff identify an issue of concern, staff should report those issues to the City through the City's 3-1-1 system so that the appropriate maintenance or administrative group can respond.

#### 4.0 INSPECTION DEFECTS

The Street and Sidewalk Hazard Inspection Policy defines hazards that will be identified as inspection defects by this inspection program. The following sections define how defects for sidewalks, pathways, crosswalks, and curbs directly abutting sidewalks and/or crosswalks shall be identified and recorded in the inspection program.

When an inspection defect could be recorded in a number of categories or against a number of assets, the defect should only be recorded once using the most appropriate inspection defect category. The inspection defect should be recorded against the asset that is the source of the defect (e.g. if there is a sharp vertical difference between a sidewalk and the top of a directly abutting curb, the defect should be recorded as a sidewalk trip when the sidewalk is out of elevation or as a curb trip when the curb is out of elevation).

##### 4.1 Sidewalks and Pathways

A sidewalk is inspected when it is improved with a permanent hard surface (such as concrete, asphalt, or pavers) that is intended for the primary use of pedestrians. A sidewalk includes the main sidewalk surface, the utility strip, accessibility curb ramps, bus stop landings, and portions of a sidewalk that cross a lane entrance when these areas are also improved with a permanent hard surface.

A pathway is inspected when it is a pedestrian or shared pedestrian path (a path that is intended for use by pedestrians and other non-motorized traffic), which is neither a roadway nor a sidewalk, which has been improved by the City with a permanent hard surface (such as concrete, asphalt, or pavers).

Sidewalks and pathways without a permanent hard surface are not inspected.

Typically, the inspector should record any observed defects at the sidewalk corner against the adjacent east or west sidewalk asset. If there is no adjacent east or west sidewalk, defects at these locations may alternately be recorded under the adjacent north or south sidewalk asset.



When a sidewalk or pathway crosses a lane entrance, the surface of the lane entrance that connects the sidewalk across the lane entrance shall be inspected as part of the sidewalk or pathway inspection.

Adjacent to the sidewalk and/or pathways there are areas that are not inspected as part of this program. Areas such as boulevards (not improved with a permanent hard surface by the City), curbs (not directly abutting a sidewalk), lanes, roadways, shoulders, trails, tree bases (not improved with an installed tree grate nor surround), and private encroachments (such as boulevard landscaping, connector and entrance walks, and stepping stones installed by an adjacent property) are not inspected for hazards as part of this program.

Inspection Defect - Description	Policy Hazard	Defect Description	Measurement
SWTrip - Sidewalk Trip	Trip	<p>A sharp vertical difference in elevation between two adjacent sections of a sidewalk surface, at a crack, or between a sidewalk surface and an abutting curb that is intended to be level with the sidewalk (when the sidewalk is out of elevation).</p> <p>Items with an intended elevation difference, such as expansion joints of structures, are not defects.</p>	More than 2.5 cm height
SWGap - Sidewalk Separation	Gap	<p>An open separation between two adjacent sections of a sidewalk surface, across a crack, or between a sidewalk surface and an abutting curb (when the sidewalk surface has moved). The measurement of the width and the depth of the gap must both be exceeded to be a defect.</p> <p>Items with an intended gap, such as expansion joints of structures and catch basins, are not defects.</p>	More than 2.5 cm width and depth
SWObstacle - Sidewalk Obstacle	Obstacles	<p>Obstacles include broken sidewalk pieces and items set in the sidewalk, such as empty sign post sleeves, bolts, and other irregularities with a sharp vertical difference in elevation from the sidewalk surface.</p> <p>Items with an intended elevation difference, such as expansion joints of structures, pole bases, steps and curbs, are not defects.</p>	More than 2.5 cm projection



Inspection Defect - Description	Policy Hazard	Defect Description	Measurement
SWTreeBase - Sidewalk Tree Base	Tree Base	<p>A defect in a tree base occurs when there is a sharp vertical difference in elevation between two adjacent sections of an installed tree grate or between an installed tree grate and a sidewalk surface (when the tree grate is out of elevation).</p> <p>Constructed gaps and openings within the surface of the tree grate and the opening between the base of the tree and the tree grate are not defects.</p> <p>When a tree grate is not installed and there is an area surrounding a tree composed of soil, granular material, plantings, and/or roots which may be uneven, these conditions are not defects.</p>	More than 2.5 cm height
XLTrip - Lane Crossing Trip	Trip	<p>A sharp vertical difference in elevation within the lane crossing surface, at a crack, between a lane crossing surface and an abutting gutter of a curb, due to the loss of surface material (e.g. pothole) or surface movement.</p> <p>Items with an intended elevation difference, such as expansion joints of structures, are not defects.</p>	More than 2.5 cm height
XLDistort - Lane Crossing Distortion	Trip	<p>A sharp vertical difference in elevation within the lane crossing surface or between a lane crossing surface and an abutting gutter of a curb, due to a distortion in the surface material.</p> <p>Items with an intended elevation difference, such as expansion joints of structures, are not defects.</p>	More than 2.5 cm height



Inspection Defect - Description	Policy Hazard	Defect Description	Measurement
XLGap - Lane Crossing Gap	Gap	<p>An open separation between two adjacent sections of a lane crossing surface, across a crack, or between a lane crossing surface and an abutting curb. The measurement of the width and the depth of the gap must both be exceeded to be a defect.</p> <p>Items with an intended gap, such as expansion joints of structures and catch basins, are not defects.</p>	More than 2.5 cm width and depth
PWTrip - Pathway Trip	Trip	<p>A sharp vertical difference in elevation between two adjacent sections of a pathway surface, at a crack, or between a pathway surface and an abutting curb that is intended to be level with the pathway (when the pathway is out of elevation).</p> <p>Items with an intended elevation difference, such as expansion joints of structures, are not defects.</p>	More than 2.5 cm height
PWGap - Pathway Separation	Gap	<p>An open separation between two adjacent sections of a pathway surface, across a crack, or between a pathway surface and an abutting curb (when the pathway surface has moved). The measurement of the width and the depth of the gap must both be exceeded to be a defect.</p> <p>Items with an intended gap, such as expansion joints of structures and catch basins, are not defects.</p>	More than 2.5 cm width and depth
PWObstacle - Pathway Obstacle	Obstacles	<p>Obstacles include broken pathway pieces and items set in the pathway, such as water meter boxes, junction boxes, manhole lids, empty sign post sleeves, bolts, and other irregularities with a sharp vertical difference in elevation from the pathway surface.</p> <p>Items with an intended elevation difference, such as expansion joints of structures, pole bases, steps and curbs, are not defects.</p>	More than 2.5 cm projection/depression



Inspection Defect - Description	Policy Hazard	Defect Description	Measure-ment
PWTreeBase - Pathway Tree Base	Tree Base	<p>A defect in a tree base occurs when there is a sharp vertical difference in elevation between two adjacent sections of an installed tree grate or between an installed tree grate and a pathway surface (when the tree grate is out of elevation).</p> <p>Constructed gaps and openings within the surface of the tree grate and the opening between the base of the tree and the tree grate are not defects.</p> <p>When a tree grate is not installed and there is an area surrounding a tree composed of soil, granular material, plantings, and/or roots which may be uneven, these conditions are not defects.</p>	More than 2.5 cm height

When a hazard condition is identified in a sidewalk or pathway through the inspection program it shall be repaired within seven (7) days, as time and resources allow.

#### 4.2 Crosswalks

A crosswalk is inspected when there is an area on the roadway where a pedestrian has the right of way when crossing a roadway that is either a marked pedestrian crossing or an area within an intersection that is the portion of a roadway between the extension of the lateral edge of the roadway and the adjacent lateral property line but does not include lane intersections.

When a hazard condition is identified in a sidewalk, either through an “annual inspection” or through a review arising from a report of a potential defect, it shall be repaired within seven (7) days, as time and resources allow.

When a crosswalk is marked the area within the marked crossing, or crossing treatment, of the roadway will be inspected.

When a crosswalk is unmarked, the area of the roadway surface that would connect the sidewalks and/or curb ramps across the roadway where a pedestrian would be expected to cross will be inspected.

When a crosswalk extends through a median, and the median has been improved with a permanent surface (such as concrete, asphalt, or pavers) to continue the crosswalk through the median, that area of the crosswalk that extends through the median will be inspected as part of the crosswalk inspection.



There are a number of locations throughout the City at intersections where pedestrians do not have the right of way to cross a roadway. These locations may be indicated through signage and/or barriers (such as guard rails, islands/medians). At these locations a crosswalk does not exist.

When a hazard is identified, it will be reported on the inspection form of a sidewalk associated with the street segment that the crosswalk is located on. If there is more than one sidewalk associated with the street segment, the inspector will record the defect on only one of the associated sidewalk inspection forms.

Inspection Defect	Policy Hazard	Description	Measurement
XWPothole - Crosswalk Pothole	Crosswalk Trip	<p>A sharp vertical difference in elevation within the crosswalk surface, at a crack, between a crosswalk surface and an abutting gutter of a curb, where there is a loss of surface material.</p> <p>Items with an intended elevation difference located within a crosswalk, such as curbs and expansion joints of structures, are not defects.</p>	More than 2.5 cm height
XWDistort - Crosswalk Distortion	Crosswalk Trip	<p>A sharp vertical difference in elevation within the crosswalk surface, at a crack, between a crosswalk surface and an abutting gutter of a curb, where there is a distortion in the surfaces.</p> <p>Items with an intended elevation difference located within a crosswalk, such as curbs and expansion joints of structures, are not defects.</p>	More than 2.5 cm height
XWGap - Crosswalk Gap	Crosswalk Gap	<p>An open separation between two adjacent sections of a crosswalk surface, across a crack, or between a crosswalk surface and an abutting gutter of a curb. The measurement of the width and the depth of the gap must both be exceeded to be a defect.</p> <p>Items with an intended gap located within a crosswalk, such as expansion joints of structures and catch basins, are not defects.</p>	More than 2.5 cm width and depth

When a hazard condition is identified in a crosswalk through the inspection program it shall be repaired within seven (7) days, as time and resources allow.

#### 4.3 Curbs Directly Abutting Sidewalks, Pathways and Crosswalks



Curbs are not inspected for defects except when they are directly abutting an adjacent sidewalk (curb walk, utility strip, curb ramp, bus stop passenger landing or lane crossing), pathway, or crosswalk. The curb will be inspected where the surface of the curb is expected to match the elevation of the abutting sidewalk, pathway, or crosswalk.

Typically, the top of curb will be inspected when there is a directly abutting sidewalk or pathway. Typically, the gutter will be inspected only when a curb is within a crosswalk or lane crossing or when the gutter is directly abutting a pathway or sidewalk.

Inspection Defect	Policy Hazard	Description	Measurement
CBTrip - Curb Trip	Curb Trip	A sharp vertical difference in elevation between two adjacent curb sections or between the curb and a sidewalk, pathway, or crosswalk surface (where the curb has moved out of elevation).	More than 2.5 cm height
CBGap - Curb Gap	Curb Gap	An open gap between two adjacent sections of a curb, across a crack, or between a curb and a sidewalk, pathway, or crosswalk surface (where the curb has moved). The measurement of the width and the depth of the gap must both be exceeded.  Items with an intended gap, such as expansion joints of structures and catch basins located within a curb are not defects.	More than 2.5 cm width and depth
CBDistort - Curb Distortion	Curb Irregularity	A sharp vertical difference in elevation within a curb or where a portion of the curb has been broken away.	More than 2.5 cm projection/depression

When a hazard condition is identified in a curb through the inspection program it shall be repaired within seven (7) days, as time and resources allow.

## 5.0 COORDINATED ACTIVITIES

Other inspection, enforcement, maintenance, and data collection activities may be undertaken by inspectors in conjunction with the Sidewalk Hazard Inspection Program and the associated data collection tools. These activities and data are not part of this inspection program, but are coordinated with the undertaking of the inspection program. Coordinated activities may differ from one inspection cycle to another.

## 6.0 SIDEWALK CONDITION RATING SCORING SYSTEM



Score	Rating	Description	Extent of Distresses
1	Very Good	As New - no distresses	0% of sidewalk area
2	Good	Minor level of distresses	<10% of sidewalk area
3	Fair	Moderate level of distresses	<25% of sidewalk area
4	Poor	Significant level of distresses	<50% of sidewalk area
5	Very Poor	Severe level of distresses	>= 50% of sidewalk area
0	N/A	No Sidewalk	N/A

A distress is a condition where there is an indication of unfavourable performance or deterioration of the sidewalk surface or structure. This would include distresses such as cracks, heaves, settlements, broken panels, surface wear, ponding, and temporary patches. Deviations within the sidewalk surface of 5 mm or less are not to be considered distresses for this rating system. The SWCR score should reflect the overall condition of each sidewalk asset for the permanent hard surfaces constructed for pedestrian use in the sidewalk area, and does not include landscaped areas, grass boulevards, tree bases, sidewalk surface encroachments (e.g. connector walks, stepping stones, etc. which are not the responsibility of the City) or any private areas/plazas abutting the sidewalk. The SWCR score entered should be reflective of the overall observed condition of the City's sidewalk at the time of inspection based on the visual observations of the sidewalk asset by the inspectors.

The data will be collected in the standard Hansen RSWhazard sidewalk hazard inspection form on the SWAssetDetails tab. SWCR scores will be entered into the "Popup Value" field on the tab and the "Text Attribute" field will be completed by selecting the value for this data collection attribute (SWCR).

On the completion of the 2015-16 Sidewalk Hazard Inspection Program, the SWCR results should be reviewed to ensure that data has been collected in accordance with the SWCR system and that the data is valid. Valid results shall then be processed and the value for the SWCR will need to be transferred to the Sidewalk Asset Inventory record in the Hansen system in the Sidewalk Inspection History table (ASSETMANAGEMENT\_STREET.SWINDHIST) as an SWCR index. The SWCR results will then be visible in Hansen for the Sidewalk Asset under the Life History tab Asset Index Value. This will require IT to process the information and update the necessary tables in the Hansen system with the results from the inspection records.

It is requested that IT makes the necessary changes to the data table values and index record values in the Hansen system to facilitate the collection of this information during the 2015-16 Sidewalk Hazard Inspection Program season.

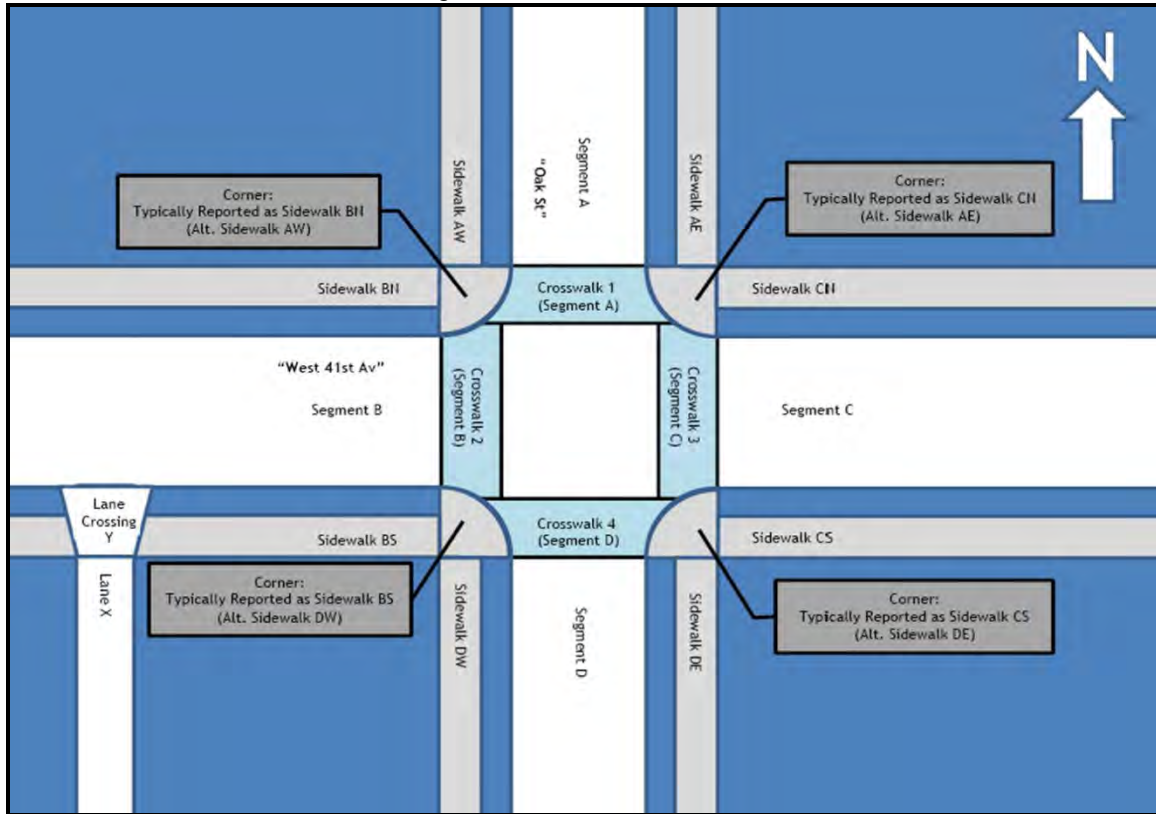
With regards to arranging for the sidewalk inspectors to collect SWCR information, we are happy to work with the inspectors to provide examples or basic training on expectations around the collection of this data. Please contact Jeff Moi if you have any further questions.



## APPENDIX A: ASSET ASSOCIATIONS

The following figure and descriptions identify how street and sidewalk assets are associated for the Sidewalk Hazard Inspection Program.

Street and Sidewalk Network Diagram



Asset and Location Associations:

Asset/Location	Associated Asset	Notes
Sidewalk AW	Street - Segment A	Sidewalk on West Side of "Oak St"
Sidewalk AE	Street - Segment A	Sidewalk on East Side of "Oak St"
Sidewalk BN	Street - Segment B	Sidewalk on North Side of "West 41st"
Sidewalk BS	Street - Segment B	Sidewalk on South Side of "West 41st"
Sidewalk CN	Street - Segment C	Sidewalk on North Side of "West 41st"
Sidewalk CS	Street - Segment C	Sidewalk on South Side of "West 41st"
Sidewalk DW	Street - Segment D	Sidewalk on West Side of "Oak St"
Sidewalk DE	Street - Segment D	Sidewalk on East Side of "Oak St"
Crosswalk 1	Street - Segment A	Crosswalk on Segment A of "Oak St"
Crosswalk 2	Street - Segment B	Crosswalk on Segment B of "West 41st"
Crosswalk 3	Street - Segment C	Crosswalk on Segment C of "West 41st"
Crosswalk 4	Street - Segment D	Crosswalk on Segment D of "Oak St"
Lane Crossing Y	Sidewalk BS	Location of Lane Crossing Y where the entrance to Lane X crosses Sidewalk BS



## APPENDIX B: HANSEN INSPECTION FORM

The following screen captures show the Hansen Inspection form used in the Sidewalk Hazard Inspection Program.

### Hazard Inspection Form - Main Page

The screenshot shows the 'Inspection Info Viewer' application running in Microsoft Internet Explorer. The browser title is 'Inspection Info Viewer - Microsoft Internet Explorer provided by the City of Vancouver'. The application has a menu bar with 'PREVIOUS', 'NEXT', 'EDIT', 'PRINT', and 'MAP DRAWER'. There are also 'HELP' and 'CLOSE' buttons. The main content area is divided into several sections:

- Inspection**: Contains fields for 'Inspection Number' (14805), 'Program' (empty), 'Inspection Type' (RSwHazard), 'Sidewalk' (100016), 'Asset Length' (0.0000), and 'Asset Width' (0.0000).
- Information**: A sidebar menu with options: 'Schedules', 'Attachments', 'Comments', 'SWHazardDetails', 'SWSegHazardDetails', and 'SWAssetDetails'.
- Schedule**: Contains fields for 'Scheduled Date' (//), 'Expire Date' (//), 'Plan' (empty), 'Responsibility' (empty), 'Assigned To' (empty), 'Group Inspection' (1000), 'Start Date' (//), 'Completed Date' (//), 'Completed By' (empty), and a 'Cancel Inspection' checkbox.
- Indexes**: A section with a tabbed interface showing 'Index Type' and 'Index'. The 'Index' tab is active, displaying 'No Information'.

### Hazard Inspection Form - Detail Page Sidewalk Hazards

This screenshot shows the 'Detail Page Sidewalk Hazards' section of the application. It features a table for recording sidewalk hazards. The table has the following columns: 'Action Item', 'Meter', 'Direction', 'Loc Id', 'Loc Num', 'Loc Other', 'Comments', 'WO Created', 'Inspector', 'Name', 'Date', and 'Responsibility'. The first row contains a green checkmark icon, a red 'X' icon, and the value 'None' in the 'Direction' column. Below the table is a 'New Item' button. The sidebar menu on the left is the same as in the previous screenshot, with 'SWHazardDetails' selected.



## Hazard Inspection Form - Detail Page Segment (Pathway) Hazards

Inspection Info Viewer - Microsoft Internet Explorer provided by the City of Vancouver

PREVIOUS NEXT EDIT PRINT MAP DRAWER HELP CLOSE

**Inspection**

Inspection Number: 14805 Program:  

Inspection Type: RSwHazard

Sidewalk 100016

Asset Length: 0.0000

Asset Width: 0.0000

**Segment Hazards**

**Hazard**

Action Item	Meters	Direction	Loc Id	Loc Num	Loc Other	Comments	WO Created	Inspector	Name	Date	Responsibility
No Information											

Information  
Schedules  
Attachments  
Comments  
SWHazardDetails  
SWSegHazardDetails  
SWAssetDetails



## APPENDIX C: COORDINATED ACTIVITIES

The following activities are currently undertaken in conjunction with the Sidewalk Hazard Inspection Program.

### *LANDSCAPING ENCROACHMENT INSPECTION*

During the undertaking of the inspection program, inspectors will check for tree branches, shrubs and hedges encroaching into pedestrian areas. These landscape encroachments limit the availability for pedestrians to use the full width of the City's sidewalk. Landscaping is encroaching when the tree branches, shrubs or hedges extend into the sidewalk area and are less than 2.4 meters (8 feet) above the sidewalk surface. Landscape encroachments may come from private properties, boulevard encroachments, or City street trees.

When conditions are observed that do not meet the requirements of the attached "Landscape Encroachment Form", the inspector shall report the location and details of the landscape encroachment by calling the City's 3-1-1 service. These locations will then be reviewed by operations staff that will issue notifications to property owners or undertake maintenance actions as may be required.



**LANDSCAPING ENCROACHMENT FORM**

Address: \_\_\_\_\_ Date: \_\_\_\_\_

Inspector: \_\_\_\_\_

**Private:** Encroaching Hedge or Shrubs ☐ Overhanging Branches ☐

Details: \_\_\_\_\_

**Park Board:** Overhanging Branches ☐

Details: \_\_\_\_\_



Date: \_\_\_\_\_

Address: \_\_\_\_\_

Dear Sir/Madam:

**HEDGE OR BRANCHES OVERHANGING SIDEWALK/LANE**

This notice concerns trees or shrubs under your care that are interfering with the use of the City sidewalk and/or lane.

As part of our street maintenance program, the City performs annual inspections of the road curbs and pavements, sidewalks, lanes and boulevards. The inspectors look for safety concerns such as sidewalk trips and deep potholes, and also record the general condition of City streets. During this inspection, and in response to complaints, we hand out this notice to owners and tenants of properties where branches or hedges overhang city sidewalks or lanes, interfering with pedestrians or vehicles.

Your property has such an encroachment and we would appreciate it if you would arrange to trim overhanging hedges or branches so that the full width of the sidewalk is available for pedestrians. All pedestrians, including seniors, people with visual impairments, people in wheelchairs or pushing strollers, or simply couples walking side-by-side, benefit from sidewalks that are clear of obstacles.

All foliage should be trimmed clear of the sidewalk, and overhanging branches should be no lower than 8 feet over the walkway. In the case of lane encroachments, these must be trimmed so as to allow vehicles, including garbage trucks, to move safely.

Please arrange to prune the growth by \_\_\_\_\_.

If you do not trim this growth the City may carry out the work and bill the cost to you. If City crews do the work, they will not have the time to give your landscaping the best care, and they may damage the health and/or appearance of your hedge or shrubs. Therefore, if you wish to ensure that your trees receive the best care, please arrange to prune back the growth yourself.

If you have any questions, or would like to discuss this matter further, please contact the Streets Operations Branch at 604.871.6213. Your co-operation in this matter would be appreciated. The nature of your encroachment is indicated below.

Foreman/Inspector: \_\_\_\_\_

Encroaching Hedge or Shrub:	<input type="checkbox"/>	onto	Sidewalk:	<input type="checkbox"/>
Overhanging Branches:	<input type="checkbox"/>		Lane:	<input type="checkbox"/>

*See sketch on reverse*



This notice contains important information which may affect you. Please ask someone to translate it for you.

此通告刊載有可能影響閣下的重要資料。請找人為你翻譯。

ਇਸ ਨੋਟਿਸ ਵਿਚ ਮਹੱਤਵਪੂਰਨ ਜਾਣਕਾਰੀ ਹੈ ਜੋ ਕਿ ਤੁਹਾਡੇ ਲਈ ਜ਼ਰੂਰੀ ਹੋ ਸਕਦੀ ਹੈ। ਕਿਰਪਾ ਕਰਕੇ ਕਿਸੇ ਨੂੰ ਇਸ ਦਾ ਉਲੱਥਾ ਕਰਨ ਲਈ ਆਖੋ।

Thông báo này có tin tức quan trọng có thể ảnh hưởng đến quý vị. Xin nhờ người phiên dịch hộ.

Este aviso contiene información importante que puede afectarle personalmente. Pídale a alguien que se lo traduzca.

Ce document contient des renseignements importants qui pourraient vous concerner. Veuillez demander à quelqu'un de vous le traduire.

